



**COLORADO**  
**Department of Transportation**

Region 4

Director's Office  
10601 10<sup>th</sup> Street  
Greeley, CO 80634

November 18, 2021

City of Boulder Landmarks Board  
2045 13th Street  
Boulder, CO 80302

MAR 07 2022

RECEIVED  
Boulder County Public Works

Re: After the Fact Post-Project Determinations of State Register Act Eligibility and Effect  
Determinations for Intersection Improvements at State Highway 7 (Arapahoe Road) and U.S.  
Highway 287 in Boulder County

Dear City of Boulder Landmarks Board:

The Colorado Department of Transportation (CDOT) invites your comments regarding the above-referenced intersection improvement project in Boulder County, Colorado, in particular the project's impacts upon historical resources. This letter and the enclosed attachments are being sent concurrently to the Colorado State Historic Preservation Officer (SHPO) as well as a number of organizations that have an interest in historic preservation in Boulder County.

We are reaching out to you as part of CDOT's post-project or "after the fact" consultation with your office regarding a recently completed developer-driven intersection improvement project in unincorporated Boulder County, which necessitated temporary construction access within the CDOT-owned right-of-way (ROW) for Arapahoe Road/State Highway 7 (Arapahoe Road). While not a CDOT designed or constructed project, CDOT was involved by issuing a temporary access permit to the developer. Due to an oversight by CDOT Region 4, the presence of a previously recorded historical resource in the project area was overlooked, and the permit was issued. The intersection project was completed in Fall 2021. In order to achieve compliance with the Colorado State Register Act, CDOT is seeking your concurrence on after-the-fact determinations of State Register of Historic Places-eligibility for all historical resources (generally  $\geq 50$  year old), and effects to these resources from the intersection project. Since CDOT's only involvement in this project was to issue an access permit to the developers, there was no federal nexus that would have triggered the need for compliance with Section 106 of the National Historic Preservation Act (NHPA).

Background

In early July 2021, the (west side of) US 287/Arapahoe (SH 7) intersection project was brought to the attention of the CDOT Region 4 Senior Historian by a private citizen and Boulder Rotary Club member, William D. Meyer, who along with a small cadre of club members formed the Nine Mile Corner Monument Working Group (Working Group). This group is and has been concerned about the condition and preservation of the stone pillars that flank Arapahoe Road on the west side of the US 287. They were conceived as memorial monuments to American dead in the "Great War" (World War I). Constructed of sandstone in 1928 by the Boulder Lions Club, these intricate structures were major features of an elaborate gateway to a proposed - but never constructed - "Road of Remembrance" leading to Boulder. A detailed history of the pillars is presented in the attached document prepared by Mr. Meyer of the Working Group.

The 93-year-old gateway pillars have faced threats to their preservation since the early 1980s. The first threat came in 1983, when the Colorado Highway Department improved and widened Arapahoe Road at and near the intersection with and the west side of US 287. The project included the relocation of the northern gateway pillar forty feet to the north, and a specialized contractor was employed to move the structure intact. Unfortunately, the effort was unsuccessful due to unstable sandy soil which caused the pillar to tumble to the ground and break apart. The Highway Department, with specialized

contractor help, faithfully reconstructed the pillar in the new location, forty feet north of its original location. The southern pillar was left in place at its original (1928) location.

In the summer of 2017, the Evergreen Development Company was pursuing construction of a large commercial development on the southwest side of the intersection. In order to accommodate a new right turn lane from eastbound SH 7 to US southbound 287, Evergreen sought approval from CDOT to relocate the south pillar out of CDOT ROW for Arapahoe Road/SH 7 and onto the developer's parcel. The CDOT Region 4 Senior Historian advised CDOT's Permits unit that, as a significant historic resource, CDOT could not allow relocating the pillar outside CDOT ROW and incorporated into a private development. The pillars' locations comprise an especially important aspect of integrity, because they were deliberately sited in 1928 to prominently define the entrance to Arapahoe Road, which was being promoted as an important route to Boulder.

Faced with the permit denial, the developers and their design consultants altered the intersection improvement project to allow for preservation in place of the southern pillar, by surrounding it with a raised and landscaped "pork chop" island (equipped with guardrail) located between a new, separated right turn lane, and Arapahoe Road.

In the recent past, CDOT's Boulder Engineer Residency discussed the idea of potentially relocating both pillars out of the Arapahoe Road ROW and place them at a future nearby planned Bus Rapid Transit (BRT) stop on Arapahoe to in order create a "sense of place" for them; however, that idea ultimately was abandoned.

#### Project Description

This developer-driven project involved improvement of the west side intersection of Arapahoe Road and US 287 in Boulder County, to provide new curved, single-lane right-in and right-out turn lanes from Arapahoe Road onto US 287 and vice versa, that would accommodate an anticipated large increase in traffic. In order to preserve the southern pillar, as required by CDOT, a "pork chop" island was built around it, separating the SH 7 roadway and the new right turn lane onto southbound US 287. The reconstructed northern pillar was not touched by the project, and has been protected from damage by a chain link fence (see enclosed 2021 inventory form prepared by CDOT). This fence was likely installed by the contractor involved in constructing the intersection improvements. Plans prepared for the recently completed intersection improvement project are enclosed for your reference.

#### Determinations of Eligibility and Effects

##### **5BL.432 - Arapahoe Road gateway pillars:**

This resource consists of two (north and south) elaborately designed and built mortared stone masonry towers or pillars flanking Arapahoe Road/SH 7 near its intersection with the west side of US 287. The pillars were envisioned and constructed as memorials to World War I American war dead, and as part of an elaborate gateway to a proposed "Road of Remembrance." The pillars are approximately 20 feet tall and are constructed of locally-quarried sandstone to intentionally replicate the appearance and general character of architecture on the University of Colorado in Boulder. Both structures are identical in design, and each has a small door leading to a small internal chamber. A cornerstone is placed at the base of the south pillar (only), while the north pillar bears a bronze dedicatory plaque. Detailed descriptions and photographs of the pillars are presented in the enclosed inventory form, and their history is explained in great detail in the enclosed document prepared by William Meyer.

Eligibility: The pillars were originally inventoried as the "Road to Remembrance Gateway" in May 1980 by Manuel M. Weiss of the Boulder County Historical Society, as part of the Boulder County Historical Site Survey, Boulder and Broomfield Counties (Report No. MC.LG.R20). Weiss prepared a Colorado Historical Society Inventory Record, and evaluated the gateway pillars as eligible for the National Register of Historic Places (NRHP). The resource has a "field eligible" determination date of May 1, 1980.

CDOT revisited, rerecorded and reevaluated the site in the summer of 2021. The reevaluation effort benefited immensely from the extensive research and detailed narrative report by the Nine Mile Corner Monument Working Group of the Boulder Rotary Club, including Mr. William Meyer, about the history of what was historically known as Nine Mile Corner (the US 287 and Arapahoe Road intersection), including the gateway pillars. They are the only remnants of the 1928 war memorial gateway that formerly included a small park with a captured German artillery piece and flagpole. Based upon an abundance of information, CDOT finds that the memorial pillars meet the eligibility criteria of the State Register of Historic Places. The site is historically significant under Criterion A for its association with a widespread movement in the 1920s led by fraternal organizations and philanthropic clubs to install public memorials to honor fallen American soldiers from the First World War. Under Criterion C, the pillars are significant for their intricate, collegiate architecture-inspired design and elaborate stonework construction.

Effect: The integrity of the pillars (5BL.432) has been greatly impacted by both the 1983 and 2021 intersection improvement projects. The 1983 Highway Department project resulted in the accidental destruction of the north pillar, and while the rebuilt structure retains the same characteristics and appearance as the intact southern pillar, it is located 40 feet north of its original location, but still closely flanking a widened Arapahoe road.

The 2021 intersection project left the southern pillar in its original location, but constructed a raised “pork chop” traffic island around it. Instead of flanking the highway, the pillar now sits between the roadway of Arapahoe Road and a new curving single-lane right turn lane that routes traffic behind the pillar - essentially destroying the spatial relationship of the south pillar and roadway. With the new improvements the south pillar no longer appears to be part of a gateway that westbound traffic on Arapahoe Road would pass through. The visual impact of these changes is severe, as the pillar no longer visually reveals its historic gateway function, and instead looks oddly out of place on the traffic island (with guardrail added).

The integrity of the pillars has also been impacted by the 40 ft relocation of the north pillar in 1983. Due to severe loss of integrity of the historic setting, CDOT has determined that the developer-driven Arapahoe Road and US 287 intersection improvement project has resulted in an *adverse effect* to 5BL.432, the Arapahoe Road gateway pillars.

#### **5BL.11936 - State Highway 7 in Boulder County (5BL.11936)**

Eligibility: Based upon the 2016 CDOT-sponsored Colorado Historic Highways Inventory by consultant Mead & Hunt, State Highway 7 was determined to be officially eligible for inclusion in the NRHP.

Effect: The project involves the Arapahoe Road/US 287 west side intersection which in the late 1920s was improved in conjunction with plans to promote Arapahoe Road as a major route to Boulder and draw more traffic to the city. The entrance to this route was designed to capture the attention of motorists and serve as a World War I memorial. Imposing tall stone pillars were erected flanking the road, and a tiny park with a WW I German trophy cannon and a flagpole was established in the space between the “wye” design of the intersection. The intersection improvements provided for two, two-way sweeping curves that funneled westbound traffic from the Lincoln Highway/US 287 past the flag and cannon, and through the narrow gap between the pillars. For eastbound traffic on Arapahoe Road, the pillars appeared as a gate on the horizon, with a cannon and flagpole strikingly centered between them. As the eastbound drivers passed between the pillars, they took one of the sweeping curves right or left to the Lincoln Highway.

Subsequently, in 1983, the Colorado Highway Department (now CDOT) redesigned and constructed the intersection to make it safer and accommodate a higher volume of traffic using both Arapahoe Road and US 287. At that time the road was widened to the north, requiring relocation of the northern gateway pillar. New turn lanes were constructed, the small war memorial park was removed, and the overall cross-section of Arapahoe Road of a short stretch of Arapahoe was widened and upgraded. The recently completed 2021 developer-driven project again redesigned the intersection, removing the 1983 intersection improvements and constructing new dedicated right-in and right-out turn lanes. The new

eastbound Arapahoe Road to southbound US 287 turn lane required the use of more land on the south side of Arapahoe within CDOT-owned ROW for Arapahoe Road. In order to preserve the south pillar within CDOT-owned ROW for Arapahoe Road, it was left in place but surrounded by a raised irregular-shaped traffic island, and the eastbound turn lane now passes behind the pillar - resulting in a major, but localized change to Arapahoe Road. The 2021 configuration is markedly different than the original 1928 design. Westbound traffic now is routed between the pillars, not on a sweeping arc but instead on a single straight lane of multi-lane highway. The curves and park are long gone, and the pillars are spread nearly twice as far apart as the original designers intended.

For eastbound traffic on Arapahoe Road, the right turn lane built in 1983 has been converted to an additional thru lane, with both thru lanes passing between the widened space between the pillars. A third lane - a new right turn lane - has been added to the south of the south pillar, so that this pillar now appears to approaching eastbound traffic as directly in the middle of Arapahoe Road.

Both the 1983 and 2021 intersection improvement projects substantially altered the alignment and width of Arapahoe Road at the intersection and extending west for approximately 510 feet (~1/10 mile) to accommodate the new turn lanes. A taper from the pre-existing roadway extends approximately 160 feet eastward from the project's western terminus, and the footprint gradually widens as the new right turn lane diverges away from the road's main thru lanes. The redesigned and constructed intersection does not bear any resemblance to the 1928 design. While the 1928 intersection has been severely altered and has lost most aspects of its historic integrity, the changes involve a very short portion of Arapahoe Road and does not substantially reduce the integrity of the entire route of Arapahoe Road/SH 7. Therefore, CDOT finds that the 2021 project resulted in a finding of *no adverse effect* with respect to this linear historic resource.

#### Proposed Mitigation

In order to mitigate the adverse effect to 5BL.432, the Arapahoe Road gateway pillars, CDOT proposes to engage with a number of stakeholders/interested parties and historic preservation agencies to cooperatively develop a plan for their preservation, monitoring and perpetual maintenance. Public access and interpretation will also be considered. CDOT will invite consulting parties to participate in a Mitigation Working Group. The Mitigation Working Group will hold meetings to discuss and develop a historic preservation plan based on the analysis of all possible preservation alternatives including the potential relocation of one or both of the pillars. CDOT will manage this effort.

Mitigation Working Group members would include the Boulder Rotary Club's Nine Mile Corner Monument Working Group, including Mr. William Meyer, who first brought this situation to our attention. Other invitees include governmental and non-governmental organizations, including the Boulder County Commissioners, the Boulder County Historic Preservation Advisory Board, the Lafayette Historic Preservation Board and the Town of Erie Historic Preservation Advisory Board. Other groups that may potentially join the working group include organizations such as the Boulder Historical Society, Historic Boulder, Inc., the Erie Historical Society and the Lafayette Historical Society. Veterans' organizations would also be invited, including American Legion Post 10 (Boulder) and Veterans of Foreign Wars (VFW) Post 1771 (Lafayette).

The working group will strive to come to a consensus regarding a preferred preservation approach for the pillars; however, if consensus is not achievable, CDOT will make a decision about the appropriate course of action based on the various alternatives considered by the Working Group. Cost estimates will be obtained for each alternative, and potential funding sources will be identified. At the culmination of the process of identifying and analyzing long-term preservation alternatives, the group will create a plan document to guide future actions. The preservation plan will be the end product that satisfies mitigation of the adverse effect to the stone pillars. The plan will provide a blueprint for preservation of the pillars, but would not include actual implementation of any restoration, maintenance work, or relocation. In terms of potential relocation, CDOT will include a plan element requiring creation of a detailed photographic record as well as detailed elevation drawings should relocation be the preferred alternative. Since the pillars remain in the Arapahoe Road ROW and therefore are owned by CDOT, the execution of the preservation plan will require close coordination with CDOT, and especially with the

CDOT Region 4 Senior Historian. We welcome your thoughts about our proposed mitigation, which we believe will facilitate preservation of this historically and architecturally significant resource.

We welcome your review of our historical resource determinations and of the proposed mitigation. If you have any questions or require additional information, please contact CDOT Region 4 Historian Jason Marmor at (970) 219-9155 or [jason.marmor@state.co.us](mailto:jason.marmor@state.co.us).

Sincerely,

*James Eussen*

James Eussen  
Region 4 Planning and Environmental Unit Manager

*Enclosures:*

- CDOT-prepared OAHP Inventory form for 5BL.432 with attachments
- 1980 inventory form for 5BL.432
- Draft "Nine Mile Corner Monument" report by William Meyer
- Plan sheets for 2021 intersection improvement project
- Right-of-Way plan sheets for 1983 intersection improvement project

Cc: Vanessa Santisteven

COLORADO CULTURAL RESOURCE SURVEY

## Architectural Inventory Form

Date \_\_\_\_\_ Initials \_\_\_\_\_

- \_\_\_\_\_ Determined Eligible- NR
- \_\_\_\_\_ Determined Not Eligible- NR
- \_\_\_\_\_ Determined Eligible- SR
- \_\_\_\_\_ Determined Not Eligible- SR
- \_\_\_\_\_ Need Data
- \_\_\_\_\_ Contributes to eligible NR District
- \_\_\_\_\_ Noncontributing to eligible NR District

### I. IDENTIFICATION

1. Resource number: **5BL432**
2. Temporary resource number: **N/A**
3. County: **Boulder**
4. City: **None; unincorporated Boulder County**
5. Historic building name: **Road of Remembrance Gateway**
6. Current building name: **Arapahoe Road gateway pillars**
7. Building address: **State Highway 7 (SH 7) /Arapahoe Road, just west of U.S. Highway 287 (North 107<sup>th</sup> Street) intersection, Boulder County**
8. Owner name and address: **Colorado Department of Transportation  
2829 W Howard Place, Denver, CO 80204**

### II. GEOGRAPHIC INFORMATION

9. P.M. **6th** Township **1N** Range **69 W**  
**NE ¼ of NE ¼ NE ¼ of NW ¼ of section 34**
10. UTM references  
North pillar: Zone **13; 4429418 mE ; 491152 mN**  
South monument: Zone **13; 4429392 mE ; 491150 mN**
11. USGS quad name: **Erie, Colorado**  
Year: **1967; Revised 1971** Map scale: **X 7.5' 15'**
12. Lot(s): **Not applicable**  
Block: **Not applicable**  
Plat: **Not applicable**  
Year Platted: **Not applicable**  
Parcel Number: **Not applicable; the north and south pillars are located in CDOT right-of-way for State Highway 7 (Arapahoe Road)**
13. Boundary Description and Justification: **The site boundary is defined as the footprints of both (north and south) pillars flanking Arapahoe Road/SH 7. Arapahoe Road itself is not included in the property boundary for the pillars, since the roadway the pillars originally flanked has been greatly altered and has no historic integrity. In 1983, the north pillar was accidentally destroyed and rebuilt. The rebuilt pillar now stands along the north side of Arapahoe Road roughly 40 feet north of its original 1928-1983 location.**

### III. ARCHITECTURAL DESCRIPTION

14. Building/structure plan (footprint, shape): **Irregular; square-plan pillars/towers with attached and projecting (2 per pillar, on opposing sides) decorative and functional stone buttress walls. At the ends of these buttress walls are low, squat square-plan stone piers that add stability to the walls and pillar structure.**

15. Dimensions in feet (pillar only): Length: **Approx. 10 ft.** x Width: **Approx. 10 ft**  
Height: **Approx. 30 ft.**
16. Number of stories: **Not applicable**
17. Primary external wall material(s): **Stone – multi-colored sandstone flagstone pieces set with mortar in coursed ashlar pattern**
18. Roof configuration: **Not applicable**
19. Primary external roof material: **Stone**
20. Special features: **Segmental stone arch lintels above doors.**
21. General architectural description: **Please refer to the attached captioned photographs. This property consists of two nearly identical tower-like pillars or monuments flanking east-west Arapahoe Road (State Highway 7) close to its intersection with the west side of North 107<sup>th</sup> Street (US Highway 287). They stand on the north and south sides of Arapahoe Road, and originally formed a formal gateway to a nine mile-long direct automobile route to Boulder from the Lincoln Highway (now US 287).**

The pillars were also associated with plans to promote and develop Arapahoe Road from the Lincoln Highway to Boulder into a proposed “Road of Remembrance” referring to the sacrifices of American soldiers in World War I. Both original pillars were erected at the same time (one after the other) and are identical in terms of design, dimensions and construction materials. They present a stately monumental and symmetrical appearance.

The pillars are constructed of locally quarried tabular pieces of multicolored sandstone (flagstone) of various sizes and thicknesses set in mortar and laid in rough courses of ashlar stonework. Each pillar structure consists primarily of a tall, square-plan tower (foundation type unknown), approximately 30 feet tall, and each is equipped with a small internal chamber sealed with small (approximately 4-5 feet high) hinge-hung painted wooden door (likely non-original). These doors are kept locked by CDOT and the chamber interiors were not accessed as part of this 2021 site re-visitation. Stone segmental arches are placed above these small doorways, using vertically oriented pieces of tabular sandstone. With the exception of one small chamber door installed on each pillar, the pillars lack openings, and display elaborate fine stonework. Defining characteristics include tall and very narrow recessed vertical bays that contain what appears to be fixed glass panes (possibly an original design feature) that were subsequently covered in brown paint. Closer inspection of these vertical bays would be necessary to identify the material installed within them, along with archival and historical photograph research.

At the base (northeast corner) of the southern pillar is a cut white limestone cornerstone with the inscribed date “1928” as well as the words “Boulder Lions Club” and the organization’s official seal. Across the road, the northern pillar has a large bronze plaque affixed to its east side. This plaque was affixed to the east side of the original pillar in 1928, and was reinstalled when the pillar was moved, damaged, and reconstructed (1983) in its current location. Placed at the formal dedication of the pillars in June 1928, the plaque’s text reads: “Road of Remembrance Gateway / In Honor of Those Who Served in the World War / Erected by the Lions Club of Boulder, 1928.”

Attached to the east end west sides of each pillar are approximately 12-14 ft-long, low coursed stone ashlar buttress walls, at the far ends of which are attached squat square-plan ashlar stone pillars, each topped by large custom-designed cast concrete scrollwork decorative elements. These walls appear to provide stability and strength to help support the pillars, and are curved

and expanded where they attach to the side of the pillar. Decorative cast concrete capital elements are placed atop the top of each buttress wall.

- 22. Architectural style/building type: **Collegiate Gothic-influenced/road gateway pillars**
- 23. Landscaping or special setting features: **The historic stone pillars are located within a formerly agricultural area of unincorporated Boulder County, and a large parcel on the south side of Arapahoe Road is former farmland that is the site of an impending construction of a large commercial development. An older small rural residential property lies on the north side of Arapahoe Road and is partially obscured from view by mature deciduous trees.**

As a result of intersection improvement projects in 1983 and 2021, Arapahoe Road was been greatly widened and slightly realigned at the junction with North 107<sup>th</sup> Street/US 287 in 1983 and 2021 to improve turning movements and increase vehicle capacity and safety. In order to preserve the south pillar in situ and still construct new right turn lanes, the 2021 project included the construction of a raised, irregularly-shaped "pork chop" traffic island surrounding the south pillar, sited between the main Arapahoe Road eastbound travel lane and the new right turn lane, which carries traffic behind and past the pillar. While the pillar was left in situ, it now sits isolated within the intersection, and its gateway purpose is not readily discernable in part because the northern pillar was relocated 40 feet north of its original location in 1983 and now sits obscured by trees. Currently the connection between the north and south pillars is not obvious.

Modern commercial and residential development in unincorporated Boulder County is transforming the agricultural landscape and modernizing the character of the area including the vicinity of the Arapahoe Road/UD 287 intersection. Former low density agricultural land is undergoing development, and this activity has greatly changed the historic setting of the Arapahoe Road stone pillars.

- 24. Associated buildings, features, or objects: **Arapahoe Road/SH 7**

#### **IV. ARCHITECTURAL HISTORY**

- 25. Date of Construction: Estimate:    Actual: **1928 (April-June) - groundbreaking and laying of cornerstone on April 18, 1928; dedicated June 17, 1928**  
Source(s) of information: **(Meyer 2021:10)**
- 26. Architect: **Meade Walter**  
Source(s) of information: **(Meyer 2021:10)**
- 27. Builder/Contractor: **Lee Roy Watson (stone mason)**  
Source(s) of information: **(Meyer 2021:10)**
- 28. Original owner: **Lions Club of Boulder**  
Source(s) of information: **(Meyer 2021:10)**
- 29. Construction history (include description and dates of major additions, alterations, or demolitions): **In 1928, architect and Lions Club member Meade Walter designed the pillars at "Nine Mile Corner" (Lincoln Highway/US 287 and Arapahoe Road intersection) as a war memorial honoring the 1,000 Boulder County residents who served in World War I, including the architect's son. The pillars closely flanked Arapahoe Road, which was much narrower than at present. The planned memorial pillars would be placed on opposite (north and south sides) of Arapahoe Road to serve as a gateway to the road to Boulder, which was proposed as a World**

War I memorial "Road of Remembrance." According to the architect, the pillars would be built of flagstone, and the design and stonework were intended to replicate "the walls and alcoves of the new University buildings."

Work on constructing the pillars took two months to complete. On April 18, 1928 ground was broken and a cornerstone was laid at the base (northeast corner) of the south pillar. Stone mason Lee Roy Watson built the multi-colored sandstone structures. Watson also is responsible for constructing some of the stone structures at St. Malo in western Boulder County possibly including the impressive stone St. Malo Church/Chapel near Allenspark, Colorado.

The completed stone pillars were dedicated on June 17, 1928. Fifty-five years later, in 1983, a Colorado Highway Department (CHD) intersection improvement project was undertaken, and involved substantial widening on the north side of Arapahoe Road. Flatiron Paving Company (Flatiron) was hired by the Highway Department to perform the intersection improvement work on Arapahoe Road. To accommodate the new intersection design, in April 1983 Flatiron subcontracted with Whitlock House Moving of Louisville to move the north pillar 40 feet to the north of its original location. While digging into the sides of the north pillar in order to jack it up and place steel beams beneath it – which were to be used to slide the pillar to its new location – the sandy soil gave way and the pillar tipped over and slowly fell to the ground and broke to pieces. Flatiron agreed to faithfully rebuild the pillar. The reconstruction plan involved cleaning the unbroken flagstone, and replacing the broken stones with flagstone of matching color. Since Meade Walter's original design plans for the pillars were not available, the damaged pillar was re-assembled and repaired to match the intact south pillar using similar construction methods and materials. Flatiron Paving hired Del's Masonry of Longmont to rebuild the pillar. The work was completed by a couple from Del's Masonry, Lorri and Roy Martinez, in the summer of 1983, and approximately 80% of the original stones were utilized. Costing around \$20,000, the effort took several weeks to finish. The south pillar remains in its original location, but in 2021 a raised "pork chop" traffic island was constructed around it.

30. Original location X (south pillar) Moved X (north pillar)  
Date of move(s): 1983

## V. HISTORICAL ASSOCIATIONS

31. Original use(s): Recreation and Culture - War memorials; Transportation – highway gateway  
32. Intermediate use(s): None  
33. Current use(s): No current use  
34. Site type(s): War memorials; road/highway gateway monuments  
35. Historical background: (Note: Please refer to the unpublished report by William Meyer (2021) for a detailed history of Nine Mile Corner including the war memorial gateway pillars. The following summary includes excerpts from this report.)

The stone pillars currently flanking Arapahoe Road just west of its intersection with U.S. 287 were originally constructed in 1928. Their construction was driven by two different motives. They were envisioned and built as part of a formal, imposing gateway to serve as war memorials flanking a proposed memorial "Road of Remembrance" (Arapahoe Road). Additionally, the gateway, of which the tall stone pillars were the primary feature, was envisioned by civic and business boosters as an effective way to divert traffic traveling on the Lincoln Highway (U.S.

**287) between Longmont and Lafayette by enticing motorists to turn west onto Arapahoe Road and visit the City of Boulder.**

In the 1910s and 1920s, when auto tourism was in its infancy, the Lincoln Highway was one of the most publicized transcontinental roads for American motorists. Businessmen and civic leaders in Boulder were interested in development of the Lincoln Highway to boost tourism and commerce. In the late 1910s and early 1920s Boulder lay ten miles west of the Lincoln Highway, with poor access to the growing commercial traffic along the Lincoln Highway. Boulder's connection to the Lincoln Highway was Arapahoe Road, which intersected the Highway at "Ten Mile Corner." Also known as the Valley Road, in the late 1910s and early 1920s, Arapahoe Road was an unpaved county road traversing the plains into the city.

As automobile use increased dramatically after World War I, the need for more and better automobile roads nationwide spurred a "Good Roads" movement, which involved local action to develop new safe automobile routes connecting communities. In 1919 the Boulder Rotary Club created a "Good Roads Committee" to pursue improving the roads in Boulder County. Working with local commercial associations in Boulder and Longmont, the committee proposed an ambitious road improvement program that included paving the entire length of the Lincoln Highway in the county, and Arapahoe Road from Ten Mile Corner to the city.

Another nationwide movement was gaining momentum concurrently: developing roads into named/designated World War I memorial travel routes, or "roads of remembrance" featuring beautification including tree plantings along the margins of the roadway as promoted by the American Forestry Association. The idea of honoring Americans who participated in World War I with roads of remembrance was championed in newspaper editorials around the country, and became a popular idea, resulting in various plans for roads of remembrance in the United States.

Boulder County embraced the road of remembrance movement, and in January 1923 the Boulder Commercial Association, a variety of the Boulder service clubs and the Boulder County Commissioners began working on the development of a "Road of Remembrance" on Arapahoe Road from Boulder to Ten Mile Corner, dedicated to the soldiers from the county who fought in WWI. The preliminary plan included construction of a gateway arch or monument at Ten Mile Corner, along with planting trees and possibly lights on both sides of the road. Boulder American Legion Post 10 quickly assumed sponsorship of Boulder County's proposed "Road of Remembrance." By 1923, Boulder's business community and civic organizations supported the plan for a "Road of Remembrance" to connect with the Lincoln Highway.

The Lincoln Highway became the main north-south road in Boulder County and from northern Colorado to Denver and carried growing tourist traffic in the 1920s. With the increase in automobile travel and the higher speeds of newer cars, the Highway's zig-zag route from Ten Mile Corner to the beginning of the pavement south of Longmont was quite dangerous. Faced with this hazardous situation, in late 1923 the Boulder County Commissioners passed a resolution intended to eliminate these dangers by relocating a portion of the Lincoln Highway (US 287) by constructing a new, straight six-mile stretch of north-south highway south of Longmont, approximately 0.75 mile west of 111th Street. The proposed new highway alignment did not follow existing roads, and required obtaining land for a right of way from farmers along the route.

Construction of the new highway began in 1926 and was completed in 1927, after which focus shifted to the proposed gateway at Nine Mile Corner. As the plan for the gateway evolved, the initial idea of a big entryway arch was abandoned, and the gateway design was changed to include a pair of stone pillars flanking Arapahoe Road, at the east end of the proposed "Road of Remembrance," as well as a small park on the triangular tract formed by the roadways. The Boulder Lions Club, which funded construction of the pillars, wanted them to "mark the new route" opened by the relocated Lincoln Highway, as well as serve as a suitable memorial in honor of those who served our country in World War I. The Road of Remembrance pillars were loosely patterned after the gateway pillars at the entrance to Lookout Mountain Park near Golden.

Plans for the pillars were drawn by Meade Walter, an architect and Lions Club member, whose son had served in the First World War. The pillars would be built of flagstone, and according to Walter it was "designed something like the walls and alcoves of the new University buildings [in Boulder]."

Ground was broken and the cornerstone for the pillars (south pillar) was laid on April 18, 1928. Construction of the pillars took two months. The work was completed by stone mason Lee Roy Watson, who is credited with constructing some of the stone structures at St. Malo in western Boulder County. The pillars were officially and ceremoniously dedicated on June 17, 1928, at which time a small box of documents and mementoes were placed in a niche in the cornerstone.

While work was proceeding on the pillars, preparations were underway for construction of the second phase of the gateway project - the triangular memorial park immediately east of the pillars. A year after the pillars were completed, Boulder American Legion Post 10 constructed the park in the middle of the "wye" at the intersection. The Legion's plan for the park centered on the captured (and deactivated) artillery pieces donated by United States government. On May 19, 1929, American Legionnaires installed a solitary 100 mm captured German cannon on a concrete base in the leased triangular tract of land, and also erected a donated 48' flagpole. Trees and shrubs were planted in the triangular plot, which was surrounded by barriers comprised of low posts and cables. The shoulder of the roadways of the arcs comprising the "wye" were sufficiently wide to permit parking for those accessing the park and the pillars.

After completion of the gateway park, support for "Road of Remembrance" project dissipated, and the planned planting of 1,000 trees along the route never came to fruition. Boulder County's interest in the "Road of Remembrance" diminished, and the concept was never fully realized. Although the war memorial pillars and adjacent small park were installed at the road's entrance, none of the other proposed amenities along the route (such as trees) were ever installed, and Arapahoe Road was never formally named or called the "Road of Remembrance."

The gateway pillars remained standing in their original locations until 1983, when the CHD improved and realigned Arapahoe Road. The intersection project expanded the highway right-of-way to the north, and required the north pillar to be relocated 40 feet to the north of its original location. While moving the north pillar it tipped over and when striking the ground broke apart. The project contractor hired a specialized company to faithfully reconstruct the north pillar.

C. 2019-2020, a development company seeking to construct a large new commercial development in the southwest quadrant of the Arapahoe Road/US 287 intersection planned

intersection improvements, including new right-in and right-out turn lanes. Knowing that CDOT would not allow the relocation of the south pillar based upon its historical significance, the developers' roadway design consultant, Galloway, chose to construct a raised "pork chop" traffic island with guard rail surrounding it. The intersection work was completed in 2021. Consequently, the south pillar now sits within Arapahoe Road rather than on the south side of and flanking the road. Since the 2021 intersection improvement project is situated within the State-owned right-of way for Arapahoe Road, CDOT provided the developers with an access permit to complete the work.

After the project was brought to the attention of CDOT's Region 4 Senior Historian (Jason Marmor) by Mr. William Meyer, on behalf of a Boulder Rotary Club working group that is seeking to ensure preservation and periodic inspection and maintenance of the pillars. In accordance with the Colorado State Register Act, CDOT conducted "after-the fact" consultation with the State Historic Preservation Officer (SHPO) and stakeholder organizations (e.g. veterans' groups and historic preservation boards) regarding the significance of the stone pillars and the effect of the 2021 intersection improvement project upon them. CDOT determined that, despite the loss of some integrity, the project caused an adverse effect that would require mitigation. CDOT proposed mitigation consisting of a collaborative process to develop a preservation plan document for the pillars, including consideration of relocation alternatives. The collaborative process would involve CDOT, the SHPO, and various interested governmental agencies, historic preservation boards, veterans' groups, and service organizations. Execution of recommendations from the preservation plan document will depend on funding, leadership and effective collaboration among the interested parties.

36. Sources of information:

Blakeslee, David Allen

1983 Warranty Deed from David Allen Blakeslee to the Colorado Department of Highways, for ownership transfer of a 0.142-acre parcel in the SE ¼ of the SW ¼ of Section 27, Township 1 North, Range 69 West, in Boulder County, Colorado. The parcel was designated Parcel No. 5 in plans for Colorado Highway Department Project No. FC-HES 007-2(5). Recorded at the Boulder County Clerk and Recorder's Office on March 7, 1983. Reception No. 536538.

Daily Times

1926 "Plans Nearing Completion for Right-of-Way Highway; Local Engineer Working on New Route." The Daily Times, Longmont, Vol. XXXII, Number 60, February 25, 1926.

Garten, Carol

1983 "Monument on Highway 7 – 287 Comes Tumbling Down." The Louisville Times, Vol. 69, No. 44, April 20, 1983.

Meyer, William

2021 Nine-Mile Corner Monument (narrative historical report), 26 pp., unpublished.

**Town of Erie**

**1983 Warranty Deed from the Town of Erie, Colorado to the Colorado Department of Highways, for ownership transfer of a 0.324-acre parcel in the NE ¼ of the NW ¼ of Section 34, Township 1 North, Range 69 West, in Boulder County, Colorado. The parcel was designated Parcel No. 6A in plans for Colorado Highway Department Project No. FC-HES 007-2(5). Recorded at the Boulder County Clerk and Recorder's Office on February 8, 1983. Reception No. 532304.**

**Weiss, Manuel**

**1980 Colorado Cultural Resource Survey Inventory Record for Site 5BL.432, "Road to Remembrance Gateway." Boulder County Historical Society, May 12, 1980.**

**VI. SIGNIFICANCE**

37. Local landmark designation: Yes ☐ No ☒ Date of designation: **Not Applicable**  
Designating authority: **Not Applicable**

38. Applicable National (and State) Register Criteria:

- ☒ A. Associated with events that have made a significant contribution to the broad pattern of our history;  
☐ B. Associated with the lives of persons significant in our past;  
☒ C. Embodies the distinctive characteristics of a type, period, or method of construction, or represents the work of a master, or that possess high artistic values, or represents a significant and distinguishable entity whose components may lack individual distinction; or  
☐ D. Has yielded, or may be likely to yield, information important in history or prehistory.

☐ Qualifies under Criteria Considerations A through G (see Manual)

☐ Does not meet any of the above National Register criteria

39. Area(s) of significance: **Social History, Architecture**

40. Period of significance: **1928**

41. Level of significance: National ☐ State ☐ Local ☒

42. Statement of significance: **The pillars were originally inventoried as the "Road to Remembrance Gateway" in May 1980 by Manuel M. Weiss of the Boulder County Historical Society, as part of the Boulder County Historical Site Survey, Boulder and Broomfield Counties (Report No. MC.LG.R20). Weiss prepared a Colorado Historical Society Inventory Record, and evaluated the gateway pillars as eligible for the National Register of Historic Places (NRHP). Weiss' 1980 inventory form does not provide a narrative explanation of the site's significance in terms of the NRHP eligibility criteria. The resource has a "field eligible" determination date of May 1, 1980.**

**In the summer of 2021 CDOT revisited, rerecorded and reevaluated the pillars. They are the only extant remnants of the 1928 war memorial gateway that formerly included a small triangular park with a captured German artillery piece and flagpole. The site is historically significant under Criterion A for its association with a widespread movement in the 1920s led by fraternal organizations and philanthropic clubs to install public memorials to honor American soldiers who served during the First World War. The pillars and gateway are also associated with enthusiastic civic promotion seeking to attract and divert early motorists from the Lincoln**

Highway/US 287 to Boulder and boost the city's economy. The tall stone pillars standing on Arapahoe Road adjacent to the Lincoln Highway were visually prominent landmarks in this sparsely populated agricultural area, and they were immediately recognized by motorists as forming a gateway to what must have appeared to be an important road.

Under Criterion C, the pillars are significant for their intricate, University of Colorado at Boulder collegiate architecture-inspired design and elaborate stonework construction. They were designed by architect Meade Walter, who does not appear to have been a prominent Colorado architect. The Arapahoe Road pillars are also architecturally significant as particularly elaborate example of stone roadway gateways. There are other somewhat similar stone pillar gateways in Colorado, such as at Lookout Mountain Park near Golden.

43. Assessment of historic physical integrity related to significance: The physical integrity of the Arapahoe Road pillars has been substantially diminished as a result of two intersection improvement projects, in 1983 and 2021. During the 1983 project the north pillar had to be relocated 40 feet to the north, but during the relocation effort it tipped over and broke apart. The pillar was immediately and expertly reconstructed in the selected location 40 feet north of its original location. Although the north pillar looks exactly like the south pillar, the separation of the pillars by 40 additional feet was a major change by creating a much wider roadway than the original road and greatly diminished their appearance as gateway markers.

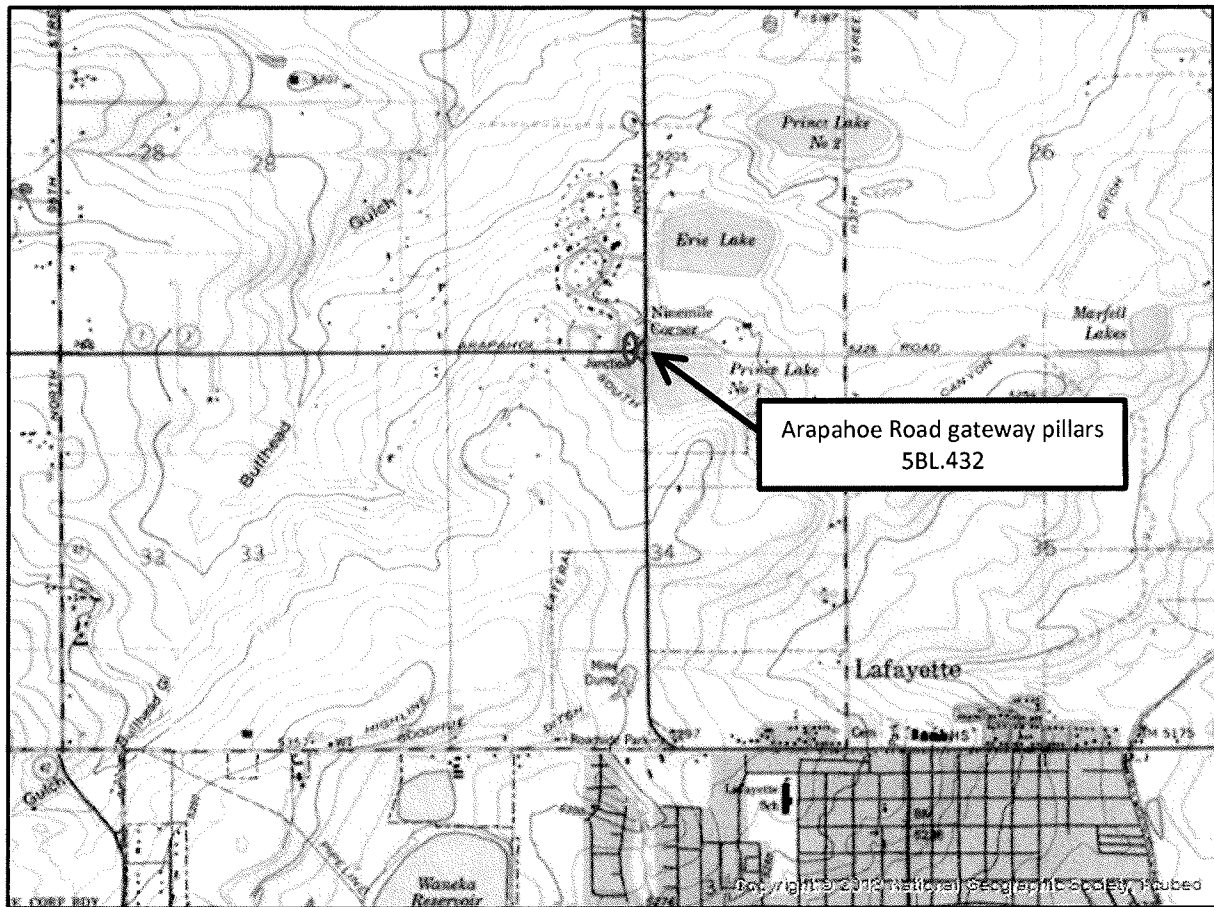
The south pillar remains intact; however recent (2021) construction greatly impacted the integrity of the historic setting - a new right turn lane was built that passes behind the pillar. Additionally, to protect the south pillar from cars, it was surrounded a raised "pork chop" traffic island equipped with guardrail; it now appears to sit within the road and adds a new and serious vehicular hazard.

## VII. NATIONAL REGISTER ELIGIBILITY ASSESSMENT

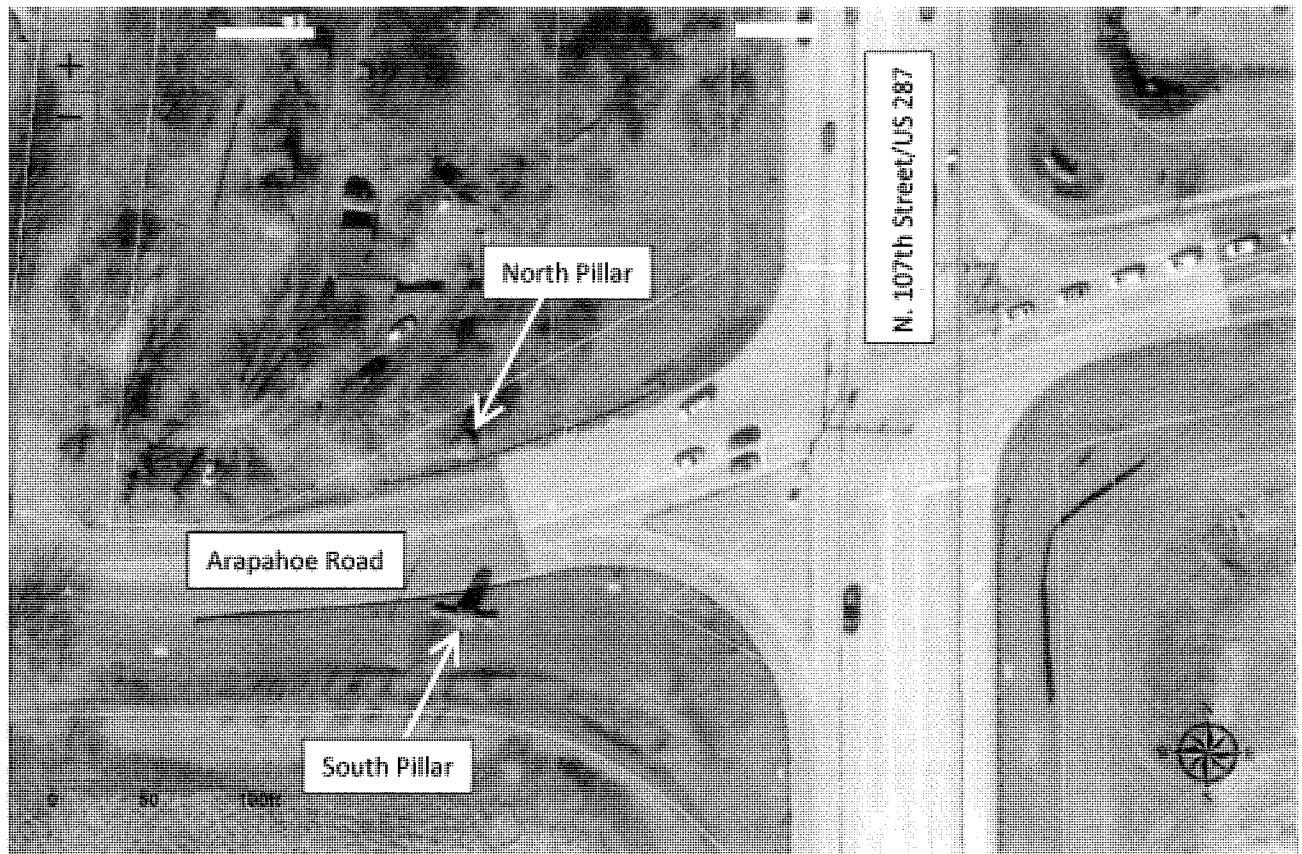
44. National Register (individual) eligibility field assessment:  
Eligible ☒ Not (Individually) Eligible ☐ Need Data ☐
45. Is there National Register district potential? Yes ☐ No ☒ Discuss: The only associated historical resource is Arapahoe Road/State Highway 7 from Nine-Mile Corner (US 287 intersection) to Boulder, which is flanked at its eastern terminus/entrance by the memorial gateway pillars. In the late 1920s this automobile road with flanking gateway pillars was conceived of as a war memorial honoring U.S. sacrifices in World War I, and was referred to as the "Road of Remembrance." Over time the commemorative name of the road was forgotten. Near the intersection with US 287, Arapahoe Road/SH 7 was widened in 1983 and even more substantially altered in 2021 to further widen and improve the intersection for safe vehicular turning movements. The 1983 intersection improvement project required the relocation of the northern pillar. The appearance and character of the highway at the location is considerably different than when the pillars were erected in 1928. Consequently Arapahoe road was not included as a contributing associated resource.  
If there is National Register district potential, is this building:  
Contributing ☐ Noncontributing ☐
46. If the building is in existing National Register district, is it:  
Contributing ☐ Noncontributing ☐ Not Applicable ☒

**IX. RECORDING INFORMATION**

- 47. Photograph numbers: **5BL.432-1 through 5BL.432-24**  
Negatives or digital photo files filed at: **Colorado Dept. of Transportation, Region 4, Greeley**
- 48. Report title: **Post-Project State Register Act Eligibility and Effect Determinations for Improvements to the U.S. Highway 287 and State Highway 7/Arapahoe Road Intersection in Boulder County.**
- 49. Date(s): **October 7, 2021**
- 50. Recorder(s): **Jason Marmor, CDOT Region 4 Senior Historian**
- 51. Organization: **Colorado Department of Transportation**
- 52. Address: **10601 West 10<sup>th</sup> Street, Greeley, CO 80634**
- 53. Phone number(s): **(970) 219-9155**



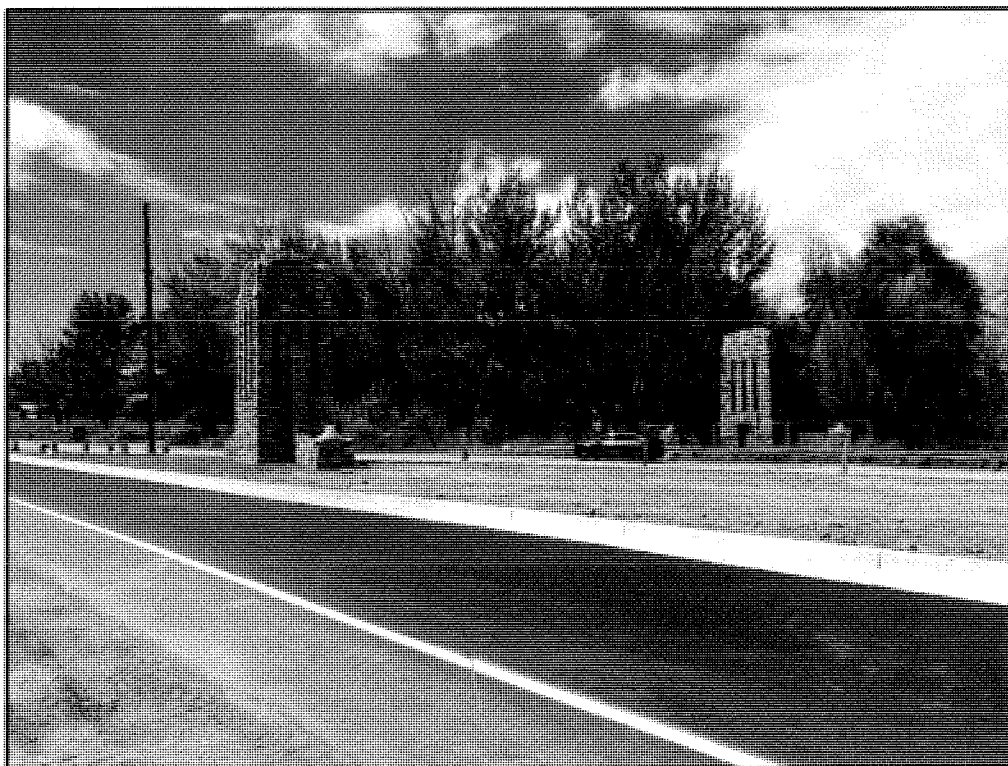
Location of the Arapahoe Road gateway pillars (5BL.432), shown on a portion of the U.S. Geological Survey 7.5' *Erie, Colorado* topographic quadrangle map (1967; revised 1971).



Sketch map of the Arapahoe Road gateway pillars (5BL.432), prior to the 2021 intersection improvement project.



Arapahoe Road gateway pillars, showing their locations and setting prior to the 2021 intersection project. View looking northwest with south pillar in foreground.



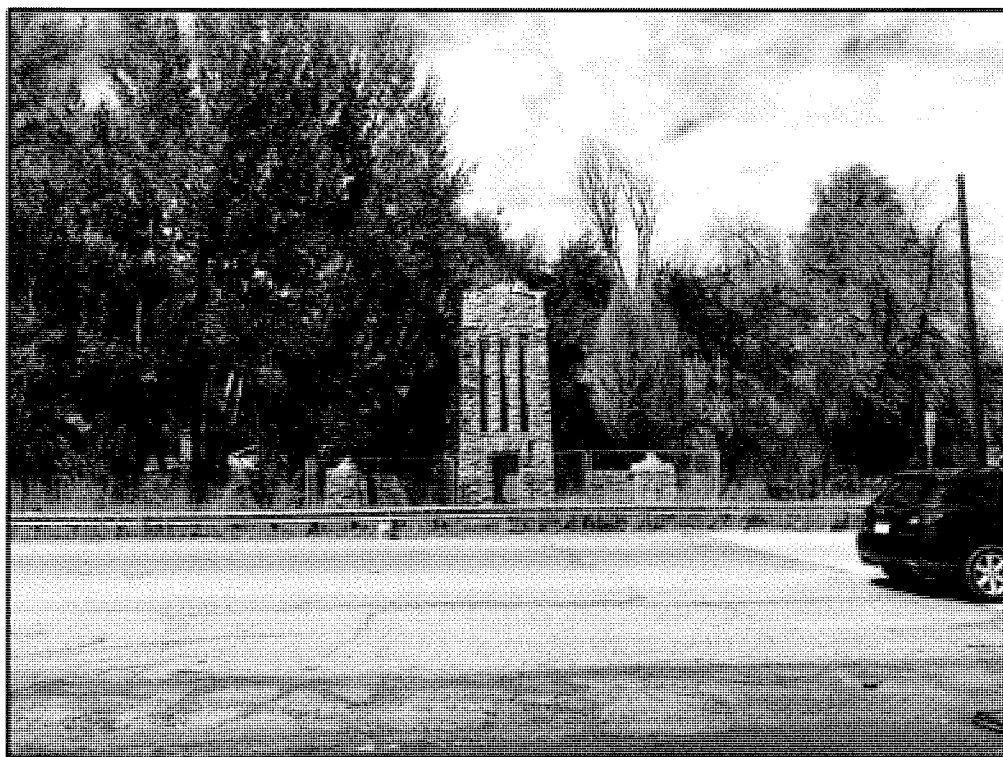
Arapahoe Road gateway pillars (5BL.432), looking west- northwest.



Arapahoe Road gateway pillars (5BL.432), looking northwest.



5BL.432, northern gateway pillar on Arapahoe Road/SH 7, looking northeast.



5BL.432, northern gateway pillar, looking north.



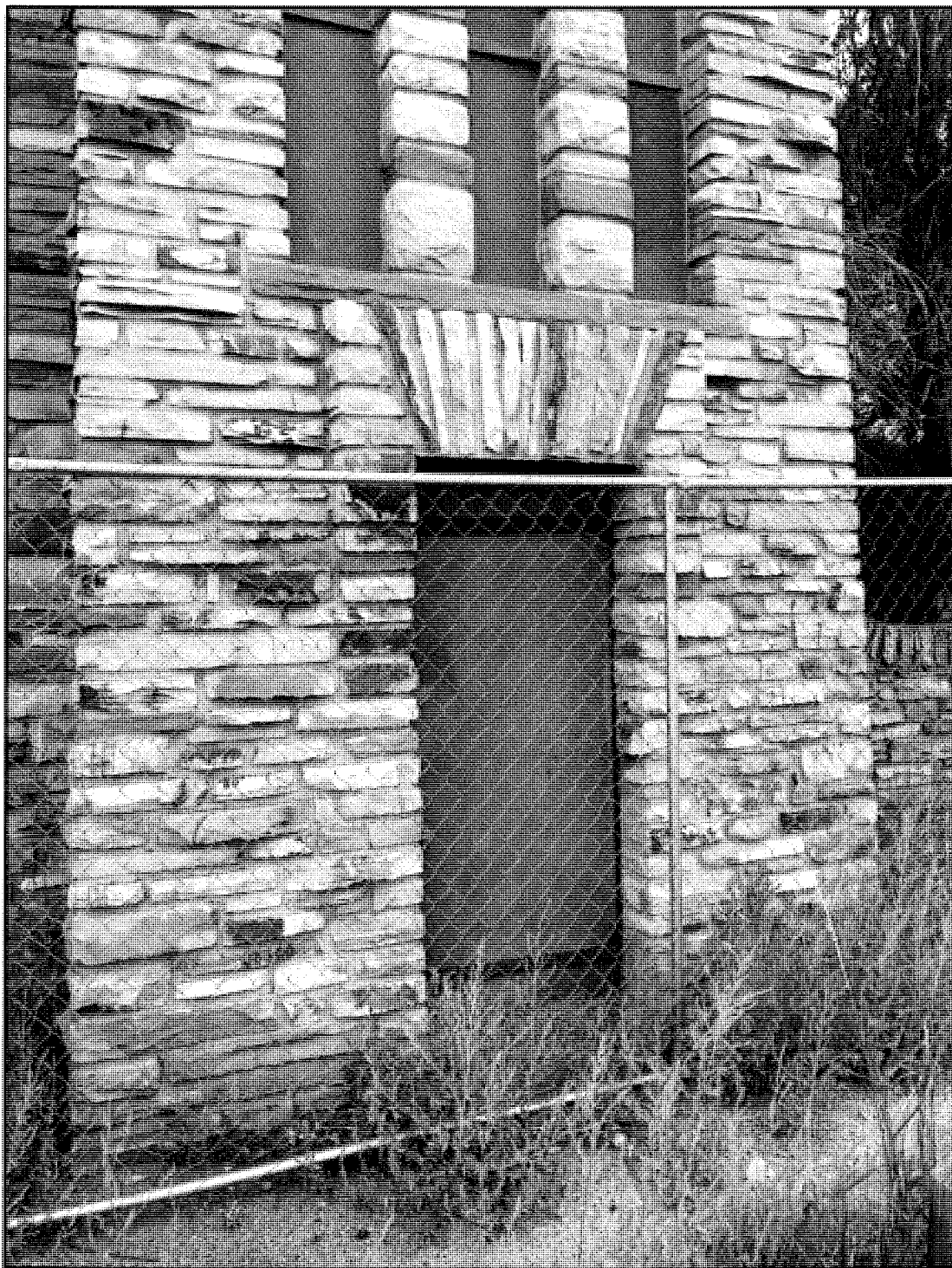
5BL.432, northern gateway pillar, looking west.



5BL.432, northern gateway pillar, looking west-northwest.



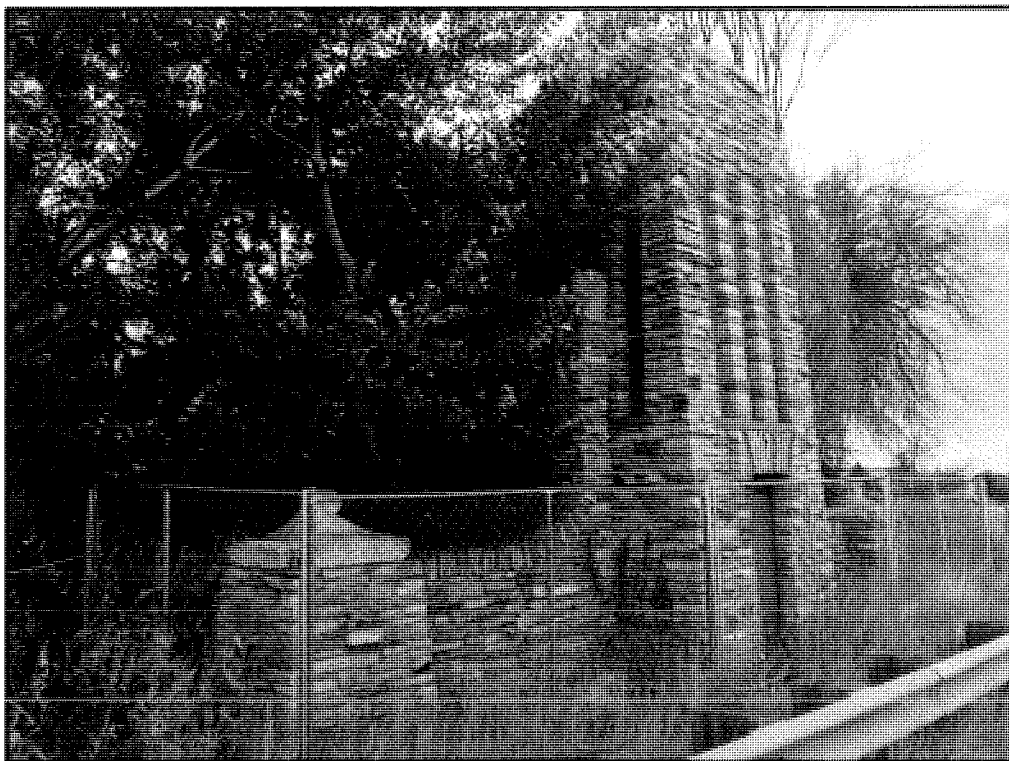
5BL.432, south side of northern gateway pillar, looking north and showing the ashlar stonework and tall, narrow window panels that are now sealed with wood.



5BL.432, door on southern face of northern gateway pillar, looking northeast.



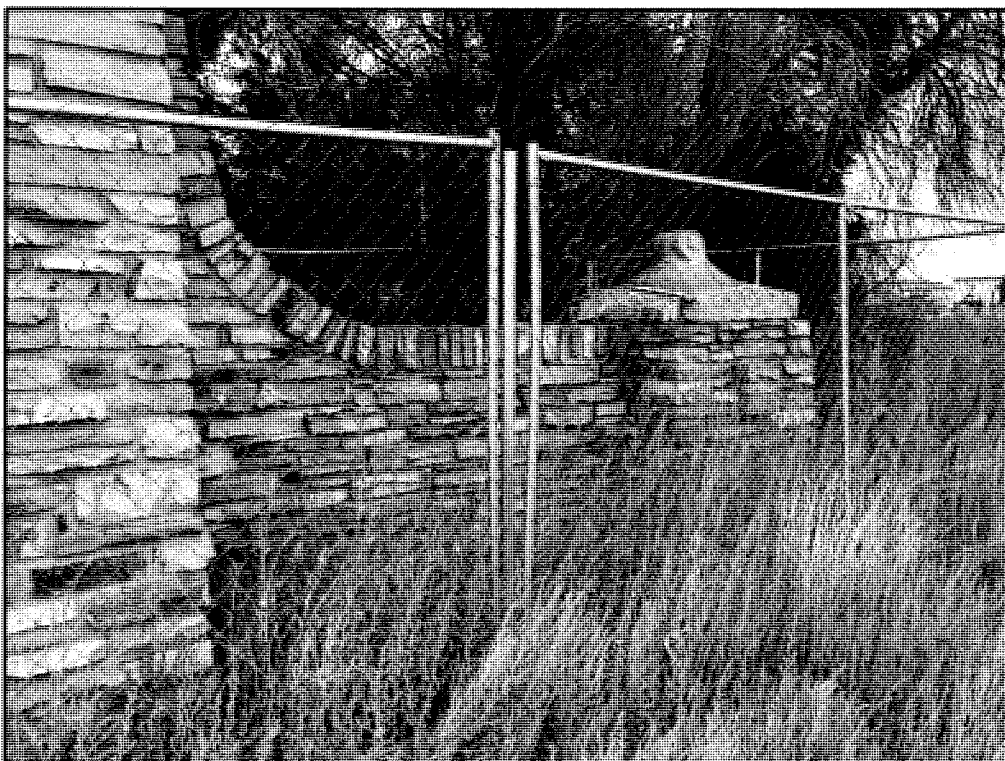
5BL.432, northern gateway pillar, looking west-southwest.



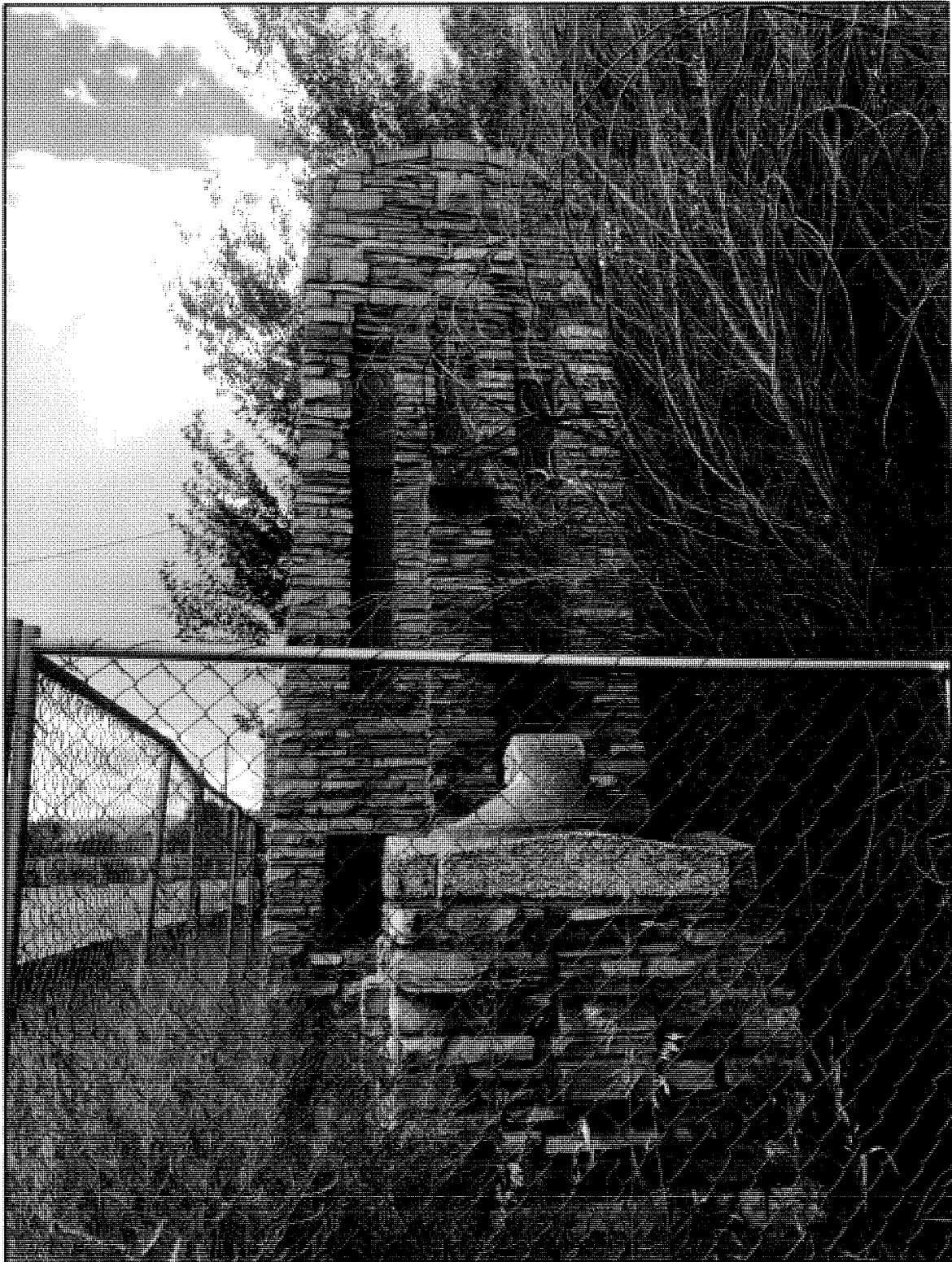
5BL.432, northern gateway pillar, looking east-northeast.



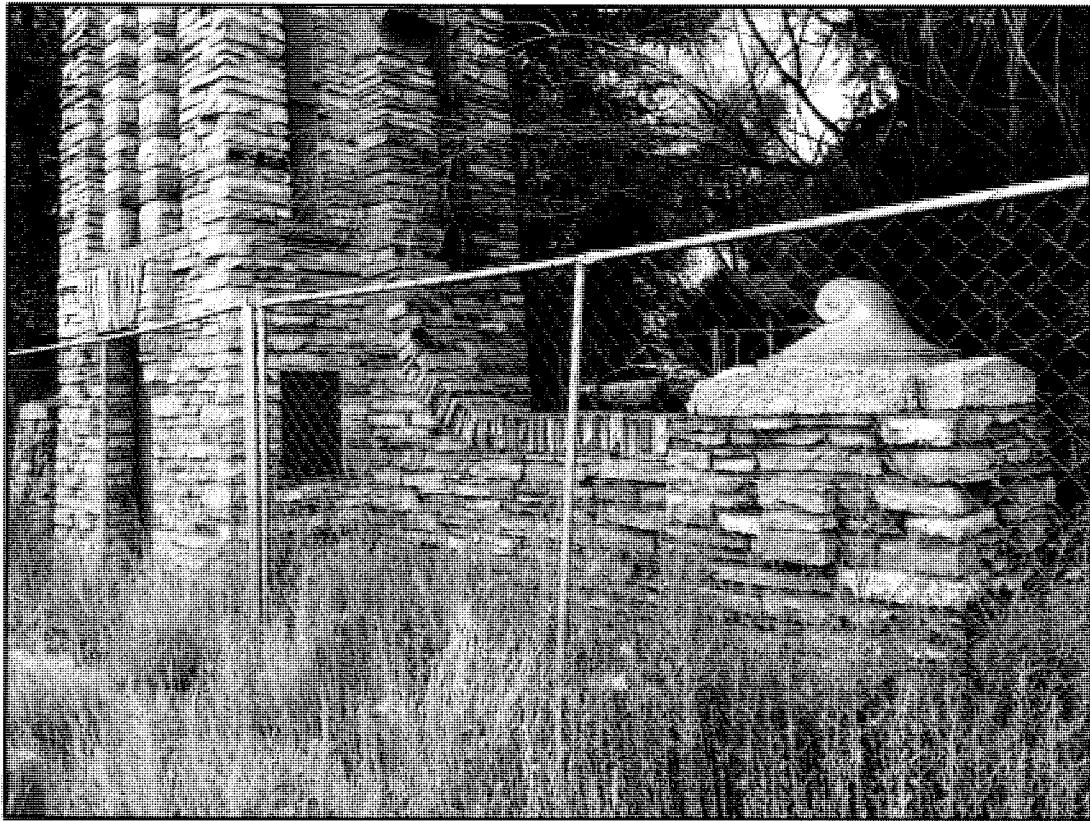
5BL.432, wall end pier topped by concrete decorative element, north pillar, looking north.



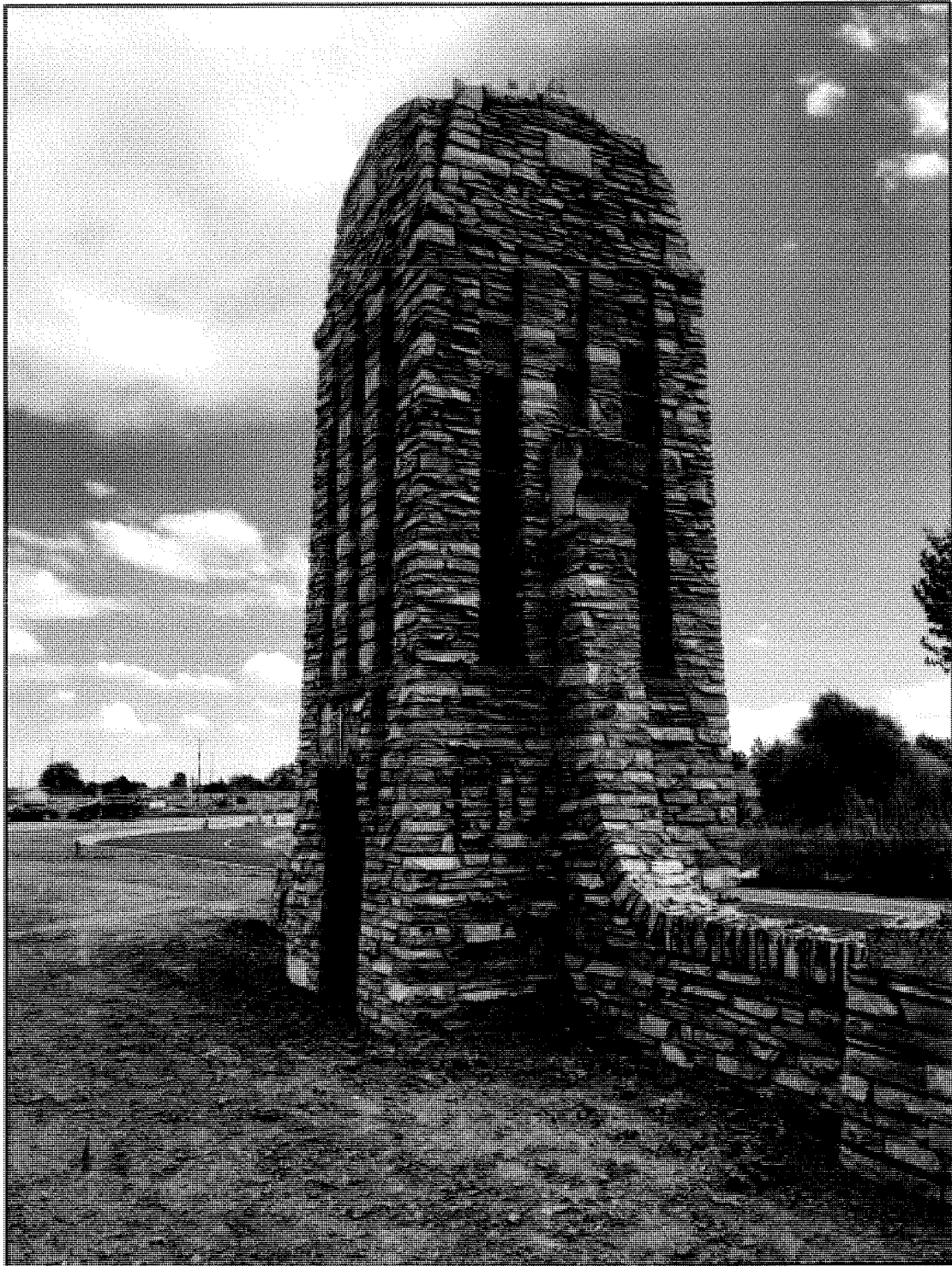
5BL.432, wall and end pier on east side of north pillar, looking northwest.



5BL.432, northern pillar, looking west, with wall end pier topped by concrete decorative element.



5BL.432, northern gateway pillar, looking northwest.



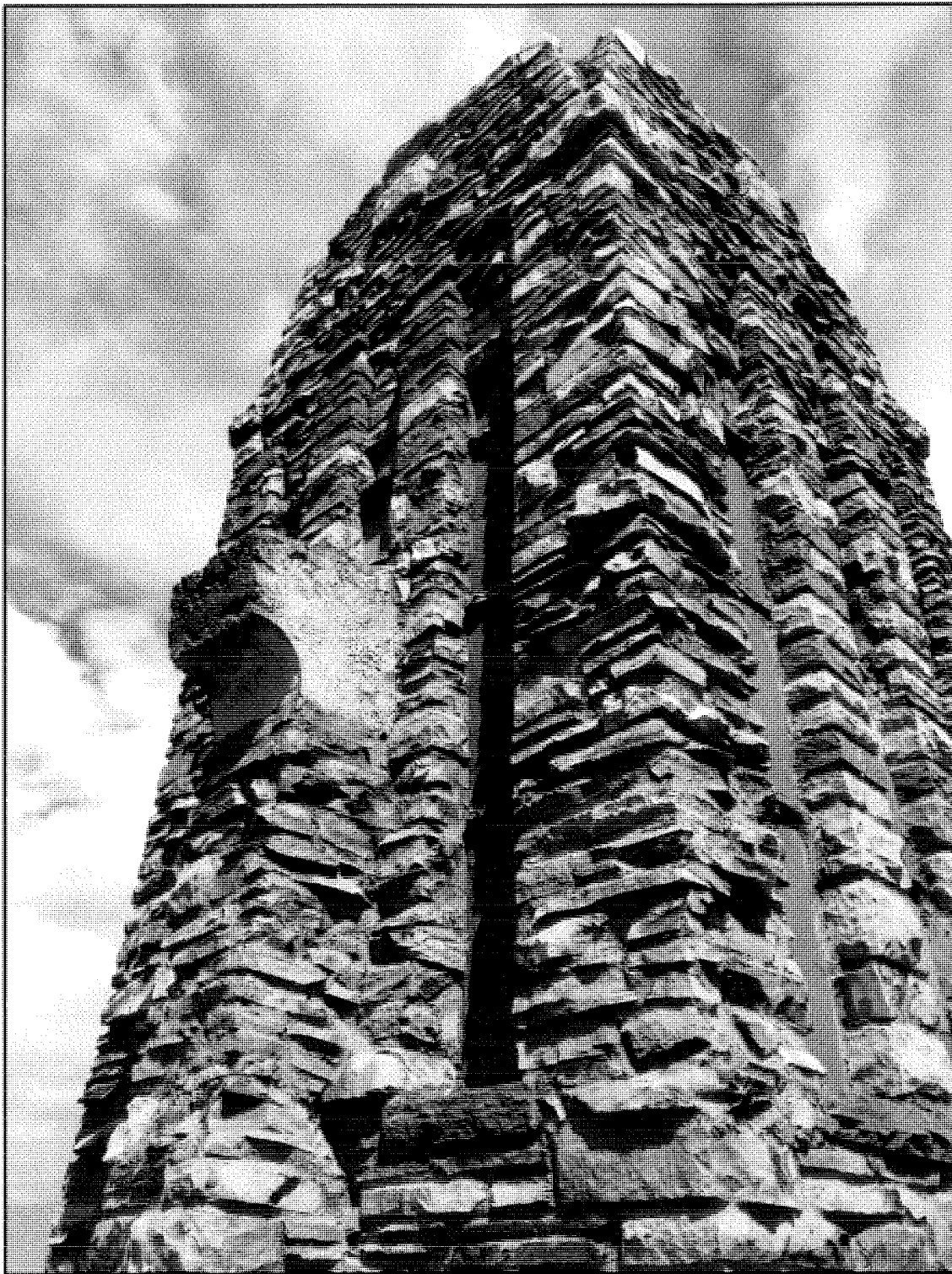
5BL.432, southern gateway pillar, looking east-southeast.



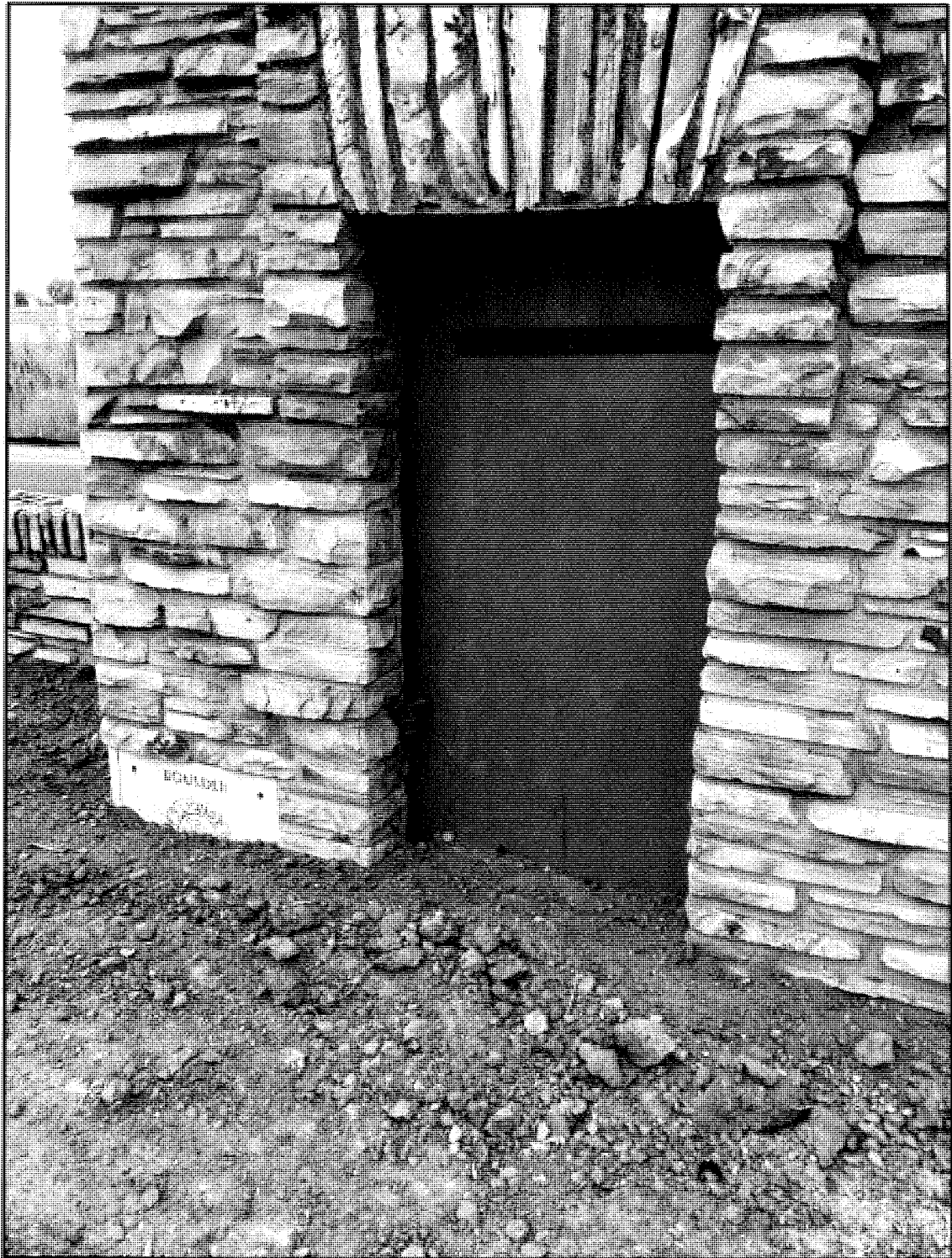
5BL.432, southern gateway pillar, looking east-northeast.



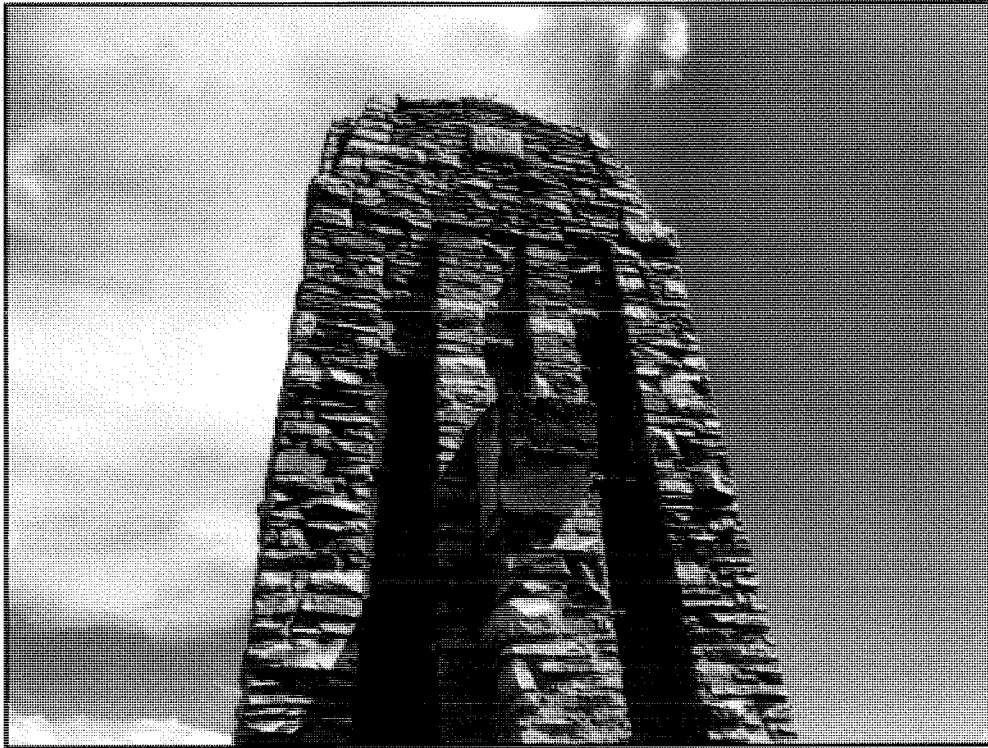
5BL.432, southern pillar, looking east.



5BL.432, close up of ashlar stonework and concrete element on southern pillar.



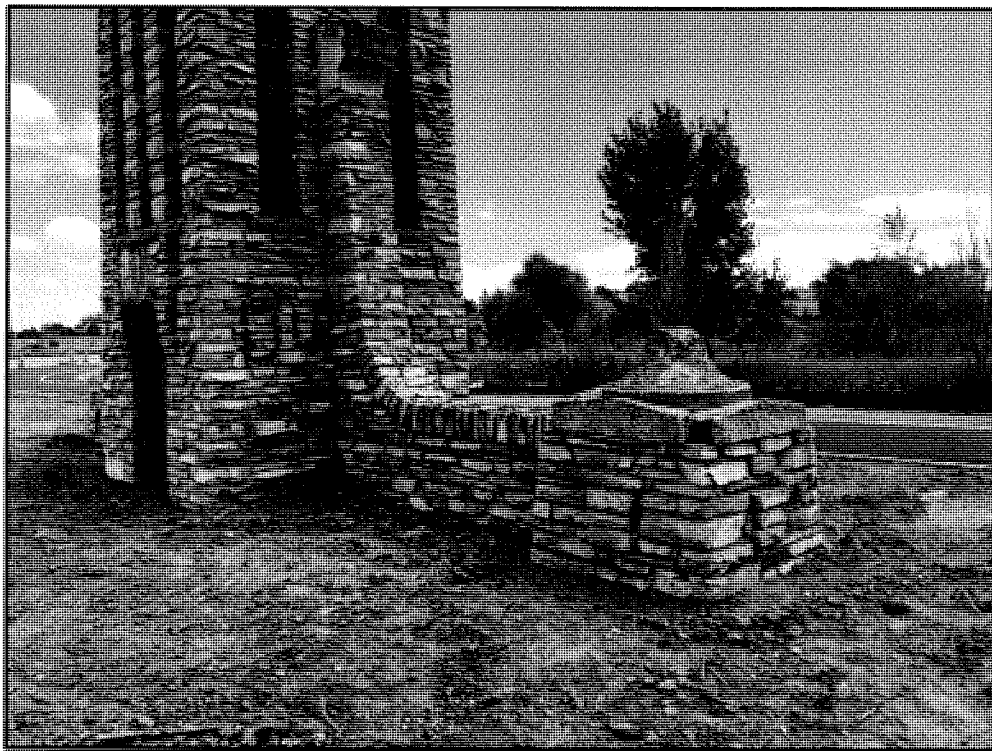
5BL.432, wooden door on north side of southern pillar, looking southeast.



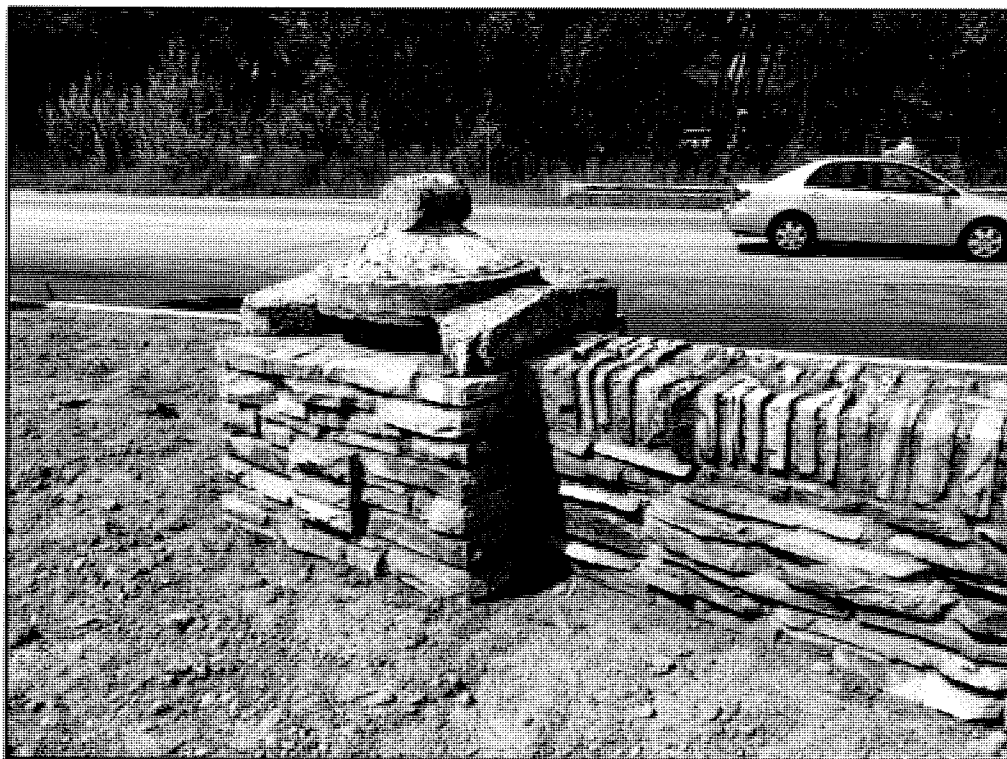
5BL.432, northern gateway pillar, looking north.



5BL.432, close-up of cornerstone at base of southern pillar.



5BL.432, west side of southern pillar, looking southeast.



5BL.432, support wall with decorative end pier of southern pillar, looking northwest.



5BL.432, southern pillar, west side pier with decorative cast concrete element, looking NNE.



5BL.432, decorative cast concrete element placed on top of end pier of southern gateway pillar, looking south.

## NINE MILE CORNER MONUMENT

Two pillars stand on Arapahoe Road at its intersection with US 287 in Boulder County. Few know that the pillars are a memorial to those who served in WWI. Fewer still know that they were built as an entrance to Boulder, or why the “entrance” was built miles from the city limits.

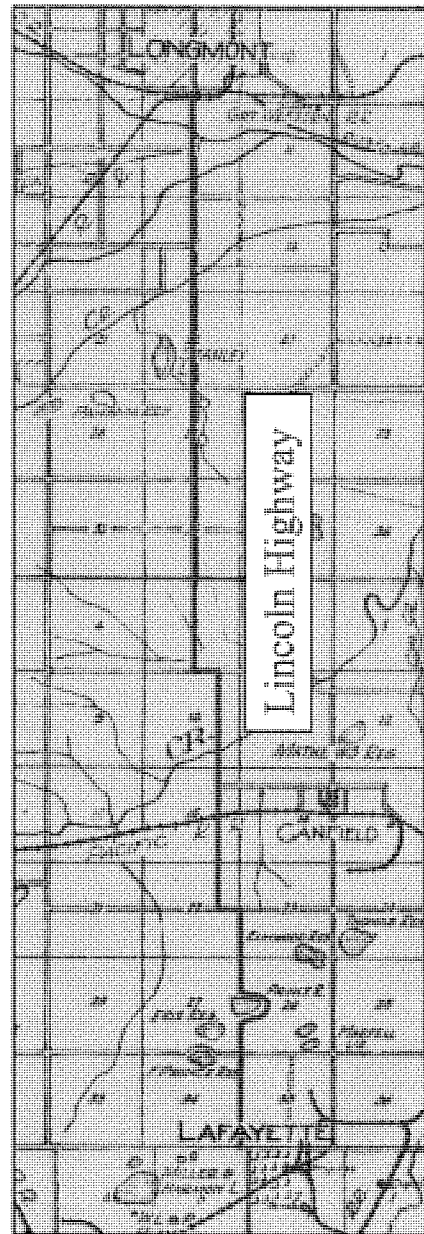
### The Lincoln Highway

The early part of the 20<sup>th</sup> century saw the rapid expansion of automobile ownership throughout the United States. With this growth came increasing demand for better roads.

In 1913, the Lincoln Highway Association was formed to promote creation of a hard-surfaced road from New York to San Francisco. While the primary route laid out by the Association’s Proclamation ran west from Omaha to Cheyenne, after furious lobbying from Colorado,<sup>1</sup> it also designated a Colorado loop heading southwest from Big Springs, Nebraska, to Denver, and then north through Longmont, Loveland, and Fort Collins, and back to Cheyenne.<sup>2</sup>

However, no “highway” existed over much of the route. To fill the void in Boulder County, local enthusiasts designated a series of unpaved county roads between Lafayette and Longmont to be the “Lincoln Highway.” This route started on the north edge of Lafayette, and proceeded north on 111<sup>th</sup> Street from its intersection with Baseline Road. After detouring around a reservoir, the route turned west on Isabelle Road, and then jogged west and north on country roads to Lookout Road, where it turned west to join 107<sup>th</sup> Street and then north to Longmont.<sup>3</sup>

In 1915, over bitter opposition from Colorado interests, the Association removed the Colorado Loop from the designated route.<sup>4</sup> Thereafter, its road guides simply noted a possible “detour” from Cheyenne to Denver along the western leg of the former Loop.<sup>5</sup> Boosters in Colorado ignored this change,<sup>6</sup> and for decades locals continued to refer to the route north from Denver to Cheyenne as the Lincoln Highway.<sup>7</sup>



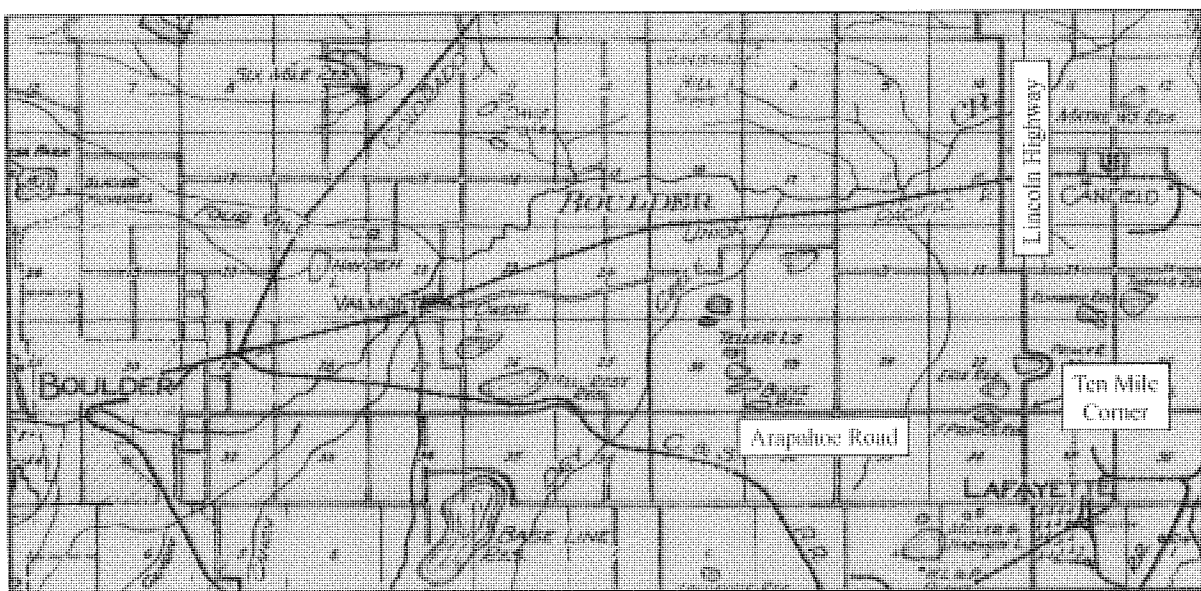
*The original Lincoln Highway* <sup>8</sup>

The creation of the Lincoln Highway coincided with another significant event in Colorado. In 1915, Rocky Mountain National Park was established outside Estes Park. Tourism had long been a significant industry in the West, with thousands of tourists arriving from the Midwest and East by train. Road trips soon began to overtake rail travel as visitors began to enjoy the mobility and freedom afforded by the automobile. By the 1920s, summer travel to Denver and north on the Lincoln Highway to Estes Park was flourishing – and lucrative for merchants along the route.

### **The Good Roads Movement and The Road of Remembrance**

One group watching the development of the Lincoln Highway were the businessmen and civic leaders in Boulder. Though it was the county seat and by far the largest town in the county, Boulder lay ten miles west of the Lincoln Highway, with poor access to the growing commercial traffic along that tourist pathway.

Boulder's connection to the Highway was Arapahoe Road, which intersected with the Lincoln Highway at the so-called Ten Mile Corner. Also known as the Valley Road, Arapahoe was an unpaved county road running across the plains into the city.



*Ten Mile Corner*<sup>9</sup>

In 1919, the Boulder Rotary Club created a “Good Roads Committee” to work on the issue of improving the roads in the area.<sup>10</sup> Working with the local Commercial Associations in Boulder and Longmont, they proposed an ambitious program to improve the County roads, including paving the entire length of the Lincoln Highway in the County, and Arapahoe Road from Ten Mile Corner to the city.<sup>11</sup>

At the same time, another roads-related movement was gaining momentum around the country. In early 1919, shortly after the end of WWI, the American Forestry Association began promoting the idea of building “roads of remembrance” to “create one vast chain of Memorial Drives that will make the country easy to see and at the same time the most famous touring country

in the world.”<sup>12</sup> Editorial writers around the country took up the cause,<sup>13</sup> and soon plans for roads of remembrance were announced around America.<sup>14</sup>

One group that quickly warmed to the road of remembrance concept was the American Legion. Organized in 1919, the Legion’s membership at that time was limited to those who served honorably between April 6, 1917, and November 11, 1918.<sup>15</sup> In 1921, the Commander of the American Legion issued a ringing call to all state departments, urging them to push memorial tree planting everywhere. Shortly thereafter, he died in an auto accident, and soon embryonic Legion posts across the country began announcing plans to help fulfill his dream.<sup>16</sup>

Another development in 1919 was the appointment of Edward B. Hill, a player in local Republican politics, as Boulder County Commissioner for District 1.<sup>17</sup> Roads in unincorporated Boulder County were controlled by the Commissioners. Under the then-existing structure, the primary duty of a county commissioner was to maintain the roads in his district. The City of Boulder, Arapahoe Road and the Lincoln Highway south of Longmont were in District 1, under Hill’s jurisdiction.

Before long, news of the road of remembrance movement reached Boulder County.<sup>18</sup> In January 1923, the Boulder Commercial Association, a variety of the Boulder service clubs and the Boulder County Commissioners began working on the development of a “Road of Remembrance” on Arapahoe Road from Boulder to Ten Mile Corner, dedicated to the soldiers from the County who fought in WWI. Inspired by Commissioner Hill,<sup>19</sup> the preliminary plan included construction of an arch or monument at Ten Mile Corner, along with planting trees and possibly lights on both sides of the road.<sup>20</sup> Money for the arch would be raised by public subscription,<sup>21</sup> and most other organizations in Boulder endorsed the plan.<sup>22</sup>

Among the first Legion posts organized in Colorado was Boulder Post 10. The newly minted Post 10 quickly took up sponsorship of Hill’s “Road of Remembrance.”<sup>23</sup> By 1923, the outlines of a plan for a “Road of Remembrance” to connect with the Lincoln Highway had the full-throated support of both Boulder’s business community and civic organizations. However, politics, money and just plain stubbornness caused five years to pass before any element of the “Road of Remembrance” could be completed.

### **Relocation of the Lincoln Highway**

Though the Lincoln Highway was a burgeoning tourist thoroughfare in the summer, it was also the main north-south road in Boulder County and from northern Colorado to Denver. With the increase in travel and the higher speeds of newer cars, the dangers of the Highway’s zig-zag route from Ten Mile Corner to the beginning of the pavement south of Longmont became apparent. Four turns, including one named “Dead Man’s Curve,” made this stretch not only dangerous to tourists, but also a hazard to local residents.<sup>24</sup>

In November 1923, about nine months after the “Road of Remembrance” project was announced, the Boulder County Commissioners passed a resolution intended to eliminate these dangers by constructing a new, straight six-mile stretch of north-south highway. The new road would be built roughly three-fourths of a mile west of 111<sup>th</sup> Street, from Baseline Road west of Lafayette to Six Mile Corner (the intersection of 107<sup>th</sup> Street and Mineral Road) south of

Longmont. However, the proposed new highway did not follow existing roads, and would require obtaining land for a right of way from farmers along the route.<sup>25</sup>

Commissioner Hill championed the relocation of the Lincoln Highway, and by December announced that the plan was about to be approved by the State. He highlighted the dangers of the existing route, and emphasized that this plan would eliminate the dangers presented by the several sharp curves on that stretch of the Highway. A newspaper report of Hill's remarks added:

Mr. Hill stated that the owners of the farms through which the road must be built have expressed a willingness to grant a right-of-way and are anxious that the highway go over the proposed route. They show no inclination to try to prevent it and most of them have stated that they want no remuneration for the land but only ask that their places be left in as good a condition as they were before the road went through and that the expense of any building or fencing moving necessary be borne by the state and county.<sup>26</sup>

Hill's optimism was either overstated or nearsighted. The plan was supported by many Boulder residents, who were keen on moving this major thoroughfare incrementally closer to Boulder. But the proposal met with significant resistance from a variety of quarters elsewhere in the County. Some farmers, led by Joseph Oscar Vaughn (J.O.V.) Wise, vigorously protested. They maintained that the new road would ruin their farms and that the expense would be prohibitive. The opponents argued that the old route was adequate, and no changes were needed.<sup>27</sup>

After weeks of debate, the relocation project was put aside. However, it was revived in 1925 when State officials indicated that state funding for paving the Lafayette-Longmont connection would not be considered until the question was settled.<sup>28</sup>

Opponents again organized to block the relocation plan. In June 1925, the Lafayette Chamber of Commerce voted to protest the change, going so far as to call for court action seeking an injunction against the county commissioners if the project proceeded.<sup>29</sup>

Leaders of the Longmont Chamber of Commerce had attended the Lafayette meeting,<sup>30</sup> and also announced that it vigorously protested the proposed change.<sup>31</sup> And within a few days, a spat erupted with the Boulder Chamber. The ostensible flashpoint was the Boulder Chamber's distribution of 40,000 tourist folders with a map showing the natural wonders in the area. The Longmonters protested: "The aged or infirmed, without the aid of a powerful finding glass, could not possibly locate Longmont on the map used by Boulder..."<sup>32</sup>

The Longmont Chamber also saw other sinister moves afoot when Hill and the Boulder Chamber's "Boulder Good Roads Committee" met privately with the State Highway Commissioners in Denver. This meeting, the Longmonters argued, came on the heels of the prior summer when "Longmont and Northern Colorado was completely isolated to tourist travel with the lack of proper road improvement on detours, until the tourist season was all over."<sup>33</sup>

In late June it was announced that the Commissioners would meet with the resident state engineer, O.R. Douglas, to go over the proposed route for the relocated Lincoln Highway, and later explain the exact route to the affected farmers. While the other Commissioners remained silent, Hill reiterated his support for the plan.<sup>34</sup>

Despite these developments, Hill continued promoting the project and working to obtain the necessary right of way.<sup>35</sup> In early July 1925, he announced that he had spent the day with Douglas, going over the proposed right-of-way for the new route. Both men indicated that the owners of the land on the south end of the route had already signed up, but that opposition continued from farmers to the north. Hill indicated that he was continuing his efforts to secure the needed right-of-way, and expressed confidence that everything would be wrapped up by September of that year.<sup>36</sup>

However, led by Wise, four farmers who controlled about 20% of the route refused to sell.<sup>37</sup> According to some reports, the other two county commissioners were not in favor of buying the new right-of-way, but instead favored rebuilding the Highway along its existing route. Hill was undaunted, and told the recalcitrant farmers that if they did not agree, he would institute condemnation proceedings.<sup>38</sup>

Presumably because of the internal battles in Boulder County, the State stepped in. In September, the State Highway Engineer announced that the new paved Lincoln Highway would follow the route championed by Hill.<sup>39</sup> On October 2, 1925, the Colorado Attorney General, acting on behalf the Colorado Highway Commission, filed a condemnation action against the obstinate landowners in Boulder District Court.<sup>40</sup>

A month later in November 1925, the Boulder County Commissioners announced that if the condemnation was approved, Arapahoe Road would be paved from its intersection with the new highway to the City of Boulder.<sup>41</sup> This project, if completed, would provide the first paved connection between Boulder and Denver.

On December 23, 1925, a jury awarded the farmers slightly more than \$11,000 for the condemned land.<sup>42</sup> Initially, the County was hesitant to accept the verdict, and contemplated pursuing an alternative route.<sup>43</sup> However, the parties soon resolved their differences and the condemnation award was confirmed.<sup>44</sup>

### **Planning Boulder's Road of Remembrance**

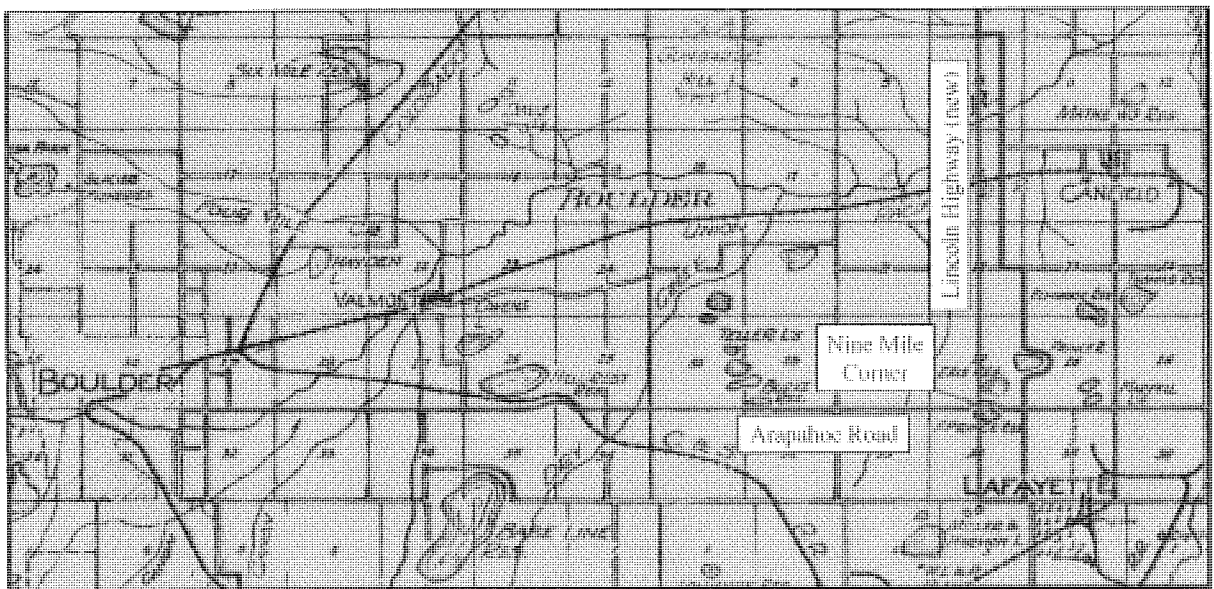
The fight over relocation of the Lincoln Highway put the plans for the "Road of Remembrance" along Arapahoe Road to Boulder on the back burner. The Legion had created a fund for the project in about 1924,<sup>45</sup> but no work had started.

While the Lincoln Highway fight raged, discussions continued about the potential Road. The original concept for the was ambitious. It called for the planting of over 1000 trees to line Arapahoe Road from Ten Mile Corner on the Lincoln Highway to Boulder. At every crossroads, grass and flowers would be planted forming miniature garden spots. At various other points, grassy parkways would be created. Several small lakes adjacent to the road would be incorporated into small parks with trees, benches, playgrounds, and row boats. Irrigation ditches would be constructed to maintain the plants. Adjacent farmers would be urged to construct attractive fences and maintain adjoining fields.<sup>46</sup>

At Ten Mile Corner, boosters continued to push for the creation of the "big arch" to entice tourists to turn toward Boulder.<sup>47</sup> The Boulder Lions Club soon took over that part of the project, and began to raise money to fund the arch's construction.

But the Legion soon added another element to the planning. In 1922, the parent American Legion began pushing Congress to release some of the nearly 2200 artillery pieces brought back as “souvenirs” by American forces returning from WWI. Local Legion posts were encouraged to lobby Congress for such trophies.<sup>48</sup> Shipment of the trophy guns began in the summer of 1925,<sup>49</sup> with Boulder’s Post 10 had receiving four captured artillery pieces.<sup>50</sup>

The relocation of the Lincoln Highway and the arrival of the war trophies opened new possibilities for the entrance way to Boulder on Arapahoe Road. Since the relocated stretch of Lincoln Highway was being built on an entirely new roadbed west of the old Highway, the new intersection would be “built from scratch.” No longer constrained by the limitations at the existing Ten Mile Corner, Hill, Douglas, and other enthusiasts began to plan a more eye-catching entrance at a spot that came to be known – for no particular cartographic reason – as “Nine Mile Corner.”<sup>51</sup>



*Nine Mile Corner* <sup>52</sup>

After the condemnation action was completed, the State and County in early 1926 began to disclose details for the design of the new Lincoln Highway. Douglas, the state’s resident engineer, was acutely aware of Boulder’s plan for Arapahoe Road.<sup>53</sup> Accordingly, its intersection with the relocated Lincoln Highway was designated to be “where the Legion Road of Remembrance will start its way west into Boulder.” The State resident engineer went on to state:

The end of Arapahoe road at this point will be spread into a wye with one arm curving north and the other curving south. The north and south road, however, will continue in a direct line, leaving a triangular park at the intersection. Here, according to tentative plans of the local Lions club and the American Legion, a memorial arch will be erected and captured German field pieces stationed.<sup>54</sup>

While the patriotic symbolism of this design was unmistakable, supporters pointed out another advantage:

The psychology back of the plan is that the casual motorist traveling the main highway will be instinctively attracted by the beautiful entrance to the road and that large numbers who would otherwise miss Boulder will turn their machine into the improved highway and follow it to the city.<sup>55</sup>

The enhanced connection to the Lincoln Highway was an increasingly important objective for the Boulder business community. In the mid-1920s, a push was underway for the federal government to identify routes along established roads across the country, and mark them with standardized number designations. The *cachet* from such official designations was expected to be boon to towns and cities on those routes.<sup>56</sup>

These designations were under discussion in 1925 and 1926, including a plan to designate a transcontinental route from El Paso to the Canadian border. This proposed route incorporated the north-south portion of the Lincoln Highway from Denver to the Wyoming border. Enthusiasts predicted that the new highway would attract millions of tourists each year to Colorado and Wyoming.<sup>57</sup>

Local leaders were thrilled by these developments. Boulder County Commissioner Guy Miller described the new stretch of highway as “part of an interstate road planned by the federal government ... [which] has been characterized as the most stupendous road building program now occupying the attention of federal road builders.”<sup>58</sup>

These few miles of road in eastern Boulder County were now, at least to local enthusiasts, part of both the legendary east-west Lincoln Highway and the soon-to-be-created north-south intercontinental federal highway. The push intensified to build the relocated Highway with an “instinctively attractive” entrance – a Gateway – to Boulder, in order to tap into the potential revenue from the increasing numbers of tourists northbound from Denver to Estes Park.

But yet another political dustup interfered. In early 1926, a battle between the Governor and the state highway department indefinitely tied up funding for road projects, including the relocation of the Lincoln Highway and the paving of Arapahoe Road.<sup>59</sup> Boulder’s dreams of a grand entrance beckoning to tourists traveling north once again were on hold.

### **The Battle at Ten Mile Corner**

The political wrangling in Denver could not have come at a worse time for the folks from Boulder. In January 1926, Hill trumpeted statistics indicating that nearly a half million autos had used the Lincoln Highway from June-September 1925, smashing previous records. Equally important for Boulder, approximately one-fourth of all of the annual traffic on the Lincoln Highway turned west on to Arapahoe Road toward the City.<sup>60</sup> With a pot of gold beckoning, Longmont and Boulder stepped up their competition. The focal point of the rivalry was Ten Mile Corner, the existing intersection of Arapahoe Road and the original Lincoln Highway.

The opening salvo was fired when the Boulder American Legion erected a large road sign near Ten Mile Corner. Precisely why the Legion was involved in the tourism business is unclear. Regardless, apparently dissatisfied with the results, the Legion next hung a banner across the Lincoln Highway reading “Boulder – Estes Park,” adorned with a large finger pointing west on Arapahoe Road.<sup>61</sup>

Heat was added to the competition when a small hut was erected at a cost of \$100 on the east side of the Lincoln Highway at Ten Mile Corner.<sup>62</sup> A large sign on the hut identified it as an information bureau and distribution station for road guides.<sup>63</sup> It was manned by “a rather genial but fluent talker” named David Fairchild, a Legionnaire who sought to persuade tourists to turn west on Arapahoe for Boulder.<sup>64</sup> According to multiple reports, Fairchild disparaged Longmont and points north, and promoted Boulder as the only town north of Denver worth seeing.<sup>65</sup>

In response, the Longmont Chamber of Commerce publicized that it had prepared a 10’ x 40’ road map showing the distances to all points of interest to be erected at Ten Mile Corner. The Longmont Chamber further boasted that it had acquired 100 new metal road signs to be erected at every crossroad north of Denver. Once erected, the Chamber crowed, no one will ever again have to ask, “Where is Longmont?”<sup>66</sup>

Apparently, Longmont’s crowing was an empty threat, and the signs never existed.<sup>67</sup> However, on the evening of July 16, 1926, the “Boulder information bureau” hut was destroyed by fire. A debate raged over the cause, with the parties variously blaming men from Longmont and Boulder.<sup>68</sup> One theory was that “it was an overabundance of Boulder ‘hot air’ which caused the conflagration.”<sup>69</sup> In any event, the hut was rebuilt by the Boulder American Legion, and Fairchild resumed his post.<sup>70</sup>

Though hard feelings continued for some time,<sup>71</sup> the Battle of Ten Mile Corner soon faded from view, as the completion of work on the new stretch of the Lincoln Highway moved the traffic west. Yet the energy directed at attracting tourists to Boulder from Ten Mile Corner, particularly by the Legion which was leading the “Road of Remembrance” project, corroborates the conclusion that patriotism was not the sole motive for the Boulder activists.

### **Construction of Nine Mile Corner**

Before construction on the Gateway could begin, the parties waited for the intersection with the new highway to be completed. After a year of infighting, the State finally approved the budget including the construction of the relocated Lincoln Highway.<sup>72</sup> Fifty percent of the funding for the new highway was provided by the federal government as part of Federal Aid Project Nos. 251 B and 281 D.<sup>73</sup>

The first order of business was acquisition of the land for the project. The four farmers who fought the highway had the rights of way on their land transferred to the State Highway Department.<sup>74</sup> With that accomplished, the remaining farmers who earlier had agreed to sell transferred their portions of the right-of-way to Boulder County.

As part of that acquisition, the County acquired additional land to accommodate the “arcs” (aka the north and south turn lanes) for the “wyé” forming the Gateway envisioned by Hill and Douglas at Nine Mile Corner. Because Arapahoe Road straddles the boundary line between Sections 27 and 34, two separate acquisitions were required. The southern half of the land required for the Gateway was part of the site of the Frank Prince Reservoir, and was acquired by the County from the Schofield Farms in April 1926.<sup>75</sup> The northern half of the land for the Gateway was purchased by the County from a farmer named Morris Spishakoff a month later in late May 1926.<sup>76</sup>

Grading for the new highway began in 1926 and was completed in 1927.<sup>77</sup> Paving at the intersection was completed in September 1927,<sup>78</sup> and Nine Mile Corner was ready for construction of the Gateway.

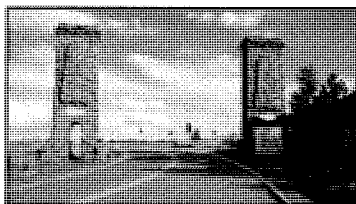
### **Building the Monument**

While work proceeded on the Highway, planning continued for the Gateway. Douglas had formally submitted his plan for roadway for the new Lincoln Highway in early March 1926. Hill triumphantly reported that this design included the “wye” at Nine Mile Corner, to be built with both state and federal funds.<sup>79</sup>

As the plan for the Gateway evolved, the idea of a “big arch” was abandoned in favor of a somewhat different design:

The junction point will form the key to the whole beautification plan. Instead of a sharp right angle, enough land will be purchased to allow construction of sweeping curves from both north and south, thus making the turn easier to negotiate. The triangle which will be formed between these two arcs and the main highway will be made into a park and seeded with grass, shrubbery and flowers. Where the two arcs meet a stone gateway somewhat similar to that which guards the entrance to the Denver Mountain Parks above Golden will be erected. This particular phase of the project will be financed by the Lions Club of Boulder.<sup>80</sup>

Thus, the Gateway at the east end of the proposed “Road of Remembrance” would have two components: (a) a small park on the triangular tract formed by the roadways, and (b) a stone gateway at the apex of the two arcs financed by the Lions Club. The press reported that the “plan has been officially endorsed by most of the organizations in Boulder and it is expected that each will contribute to the financial outlay necessary to place it in effect.”<sup>81</sup>



*Lookout Mountain Park*

For their part, the Lions Club wanted its stone structure to “mark the new route” opened by the relocated Lincoln Highway,<sup>82</sup> as well as be a “suitable memorial in honor of those who served our country in the great World War.”<sup>83</sup> Loosely patterned after the pillars at the entrance to Lookout Mountain Park near Golden, the Monument would consist of two pillars flanking the roadway.

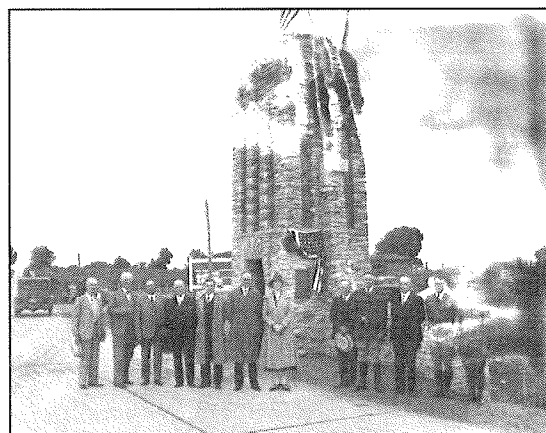
Plans for the Monument were drawn by Meade Walter, an architect and Lions Club member, whose son had served in the War.<sup>84</sup> The planned Monument would be built of flagstone, and “designed something like the walls and alcoves of the new University buildings.”<sup>85</sup> A three-man committee of Boulder Lions composed of Frank Henderson, Charles Cleveland and former Boulder Mayor James Brillig spearheaded the project.<sup>86</sup>

Precise original locations of the Monument pillars are presently unknown. However, the pillars as originally constructed flanked Arapahoe Road, which straddles the line between Sections 27 and 34. Accordingly, it is likely that the north pillar was originally constructed in Section 27 on land acquired by the County from Spishakoff. Similarly, it appears probable that the south pillar was built in Section 34 on land acquired by the County from Schofield Farms.

The groundbreaking and laying of the cornerstone for the Monument occurred on April 18, 1928. Approximately 200 officials and citizens gathered at Nine Mile Corner for the occasion. Joining Lions Club officials was Commissioner Hill, the godparent of the project. Members of the Legion and Legion Auxiliary, and representatives of the City of Boulder and the Boulder public schools attended. As part of the ceremony, a small metal box containing documents and mementos was placed by the Lions Club in a notch in the cornerstone.<sup>87</sup> No mention was made in the press reports of any attendees from Longmont or Lafayette.

Construction of the Monument took two months, with the stonework laid by Lee Roy Watson, a mason who also built some of the structures at St. Malo in western Boulder County.<sup>88</sup>

The Monument was dedicated on June 17, 1928, in a ceremony attended by more than 1,000 people, with music by the Boulder City Band and speeches from numerous dignitaries. The Dedication was described as “a beginning of the program that Boulder has adopted thru the Legion, of making Arapahoe road from the city limits to Nine Mile Corner, a Road of Remembrance.” During the ceremonies Cleveland, on behalf of the Lions, “formally presented the Gateway to the Legion.” City of Boulder Mayor, L.W. Cumberland, also “accepted the gateway in behalf of the city.”<sup>89</sup>



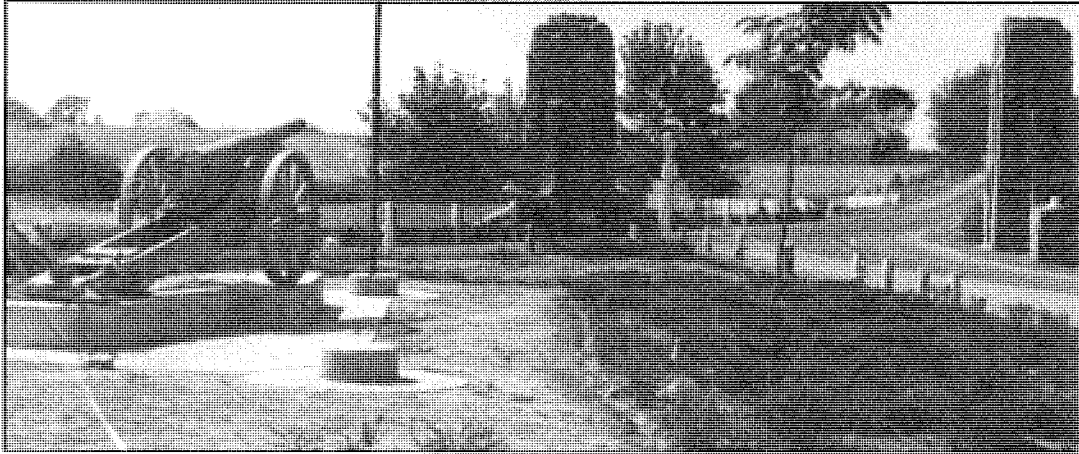
*Dedication June 17, 1928*

### **The Gateway Park**

While work was proceeding on the Monument, preparations were underway for construction of the second phase of the Gateway project, the triangular park immediately east of the Monument. By 1927, the Legion had raised over \$1000 for the project.<sup>90</sup> On December 30, 1927, Boulder County leased the ground for the park to the trustees for Boulder Post 10 for 99 years with rental payments of \$5.00 per year. The tract was described as “formed by the intersection of Federal Aid Project Road No. 251 B and Federal Aid Project Road No. 281 D.”<sup>91</sup>

A year after the Monument was completed, Boulder Post 10 built the park in the middle of the “wye” created by Douglas’ design. The Legion’s plan for the park centered on the captured artillery pieces received from United States government. Initially, it was unclear whether more than one of the guns would be placed in the park.<sup>92</sup>

On May 19, 1929, twenty-five Legion members bolted a single 100 mm captured German cannon<sup>93</sup> to a concrete base in the leased triangular tract of land.<sup>94</sup> The Legion also erected a donated 48’ flagpole, and arranged for a nearby neighbor raise and lower the American flag each day. The following week, they graveled the area.<sup>95</sup> Trees and shrubs were planted in the triangular plot, which was surrounded by barriers comprised of low posts and cables. The shoulder of the roadways of the arcs comprising the “wye” were sufficiently wide to permit parking for those accessing the park and the Monument.

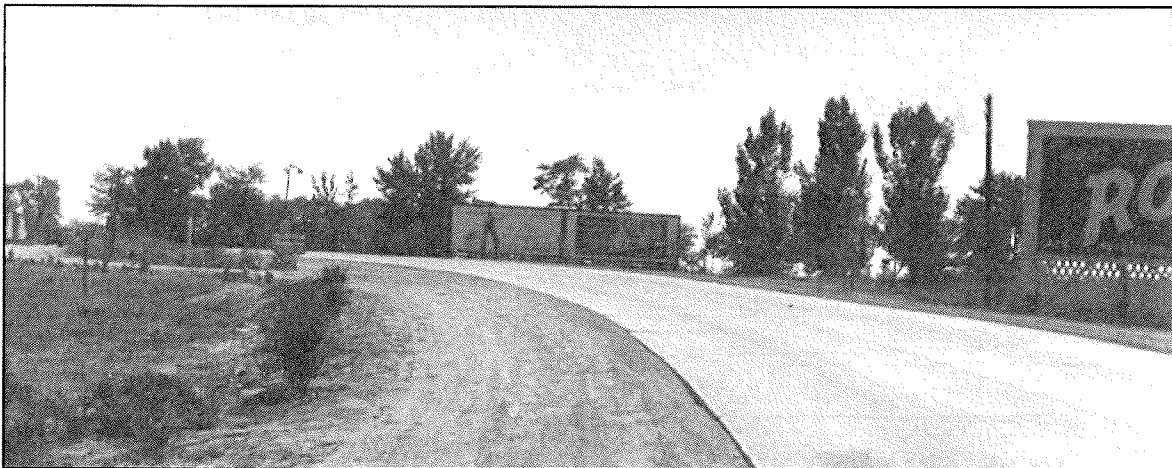


*Gateway, date unknown*<sup>96</sup>

### **The Decline of the Road of Remembrance**

Despite the noble sentiments espoused by the sponsors of the Gateway, the commercial competition between the various cities on the Lincoln Highway shifted from Ten Mile Corner to the new entrance to Boulder. Before the first shovel of dirt was turned for the Monument, Spishakoff, who had just sold land to the County for the north “arc” of the Gateway, began leasing land on his adjacent property near the intersection to out-of-state interests for the erection of “hideous and huge billboards.” Though the Boulder Lions protested, the County Commissioners admitted that they were helpless to stop the activity.<sup>97</sup>

Yet the folks from Boulder were hardly immune to commercialization at Nine Mile Corner. A primary motivation for the “beautification” of Nine Mile Corner was to entice tourists to turn west toward Boulder. Before the Monument was even built, plans were announced by the Boulder Chamber of Commerce in the spring of 1928 to erect their own 10’ x 40’ billboards at the intersection to direct tourists to Boulder. Other cities along the route threatened to do the same.<sup>98</sup> While it is unclear whether these billboards were ever built, Nine Mile Corner became home to numerous billboards.



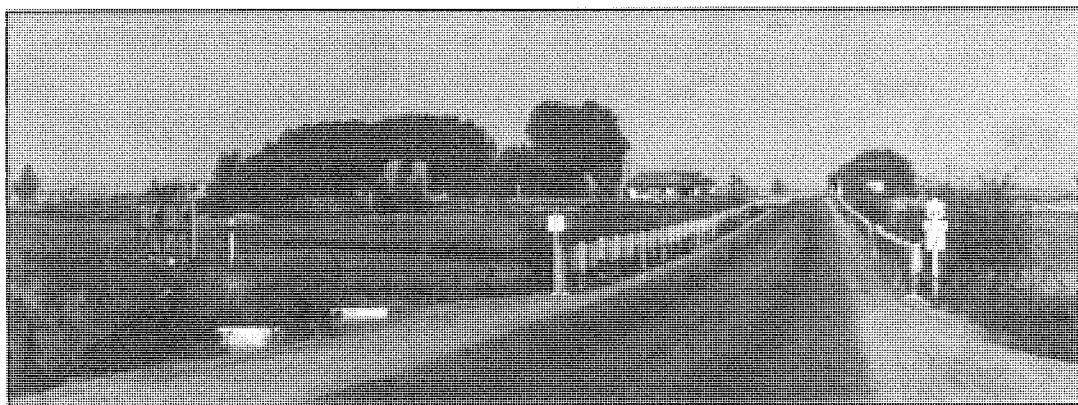
*Nine Mile Corner, date unknown*<sup>99</sup>

Later, to ensure that travelers did not miss the opportunity, three Boulder service clubs erected a large sign at the intersection unmistakably directing them to the city.



*Nine Mile Corner, date unknown*<sup>100</sup>

Much to the displeasure of the Boulder sponsors, commercialization of Nine Mile Corner continued. In 1928, Spishakoff sold a small parcel abutting the Lincoln Highway on the entrance to the northern arc to developers,<sup>101</sup> and a gas station and lunch stand operated by Lennie Herring were soon erected at that spot.<sup>102</sup> The Boulder Lions Club protested, contending that “it detracts much from the appearance of the beautiful gateway that they have erected.”<sup>103</sup>



*Nine Mile Corner, date unknown*<sup>104</sup>

With completion of the Gateway Park, the energy for the “Road of Remembrance” project among the various service clubs began to falter. The 1000 trees were never planted, though during the summer of 1928, Legion members planted hollyhock and poppy seed near streams and irrigation ditches near Arapahoe Road.<sup>105</sup>

Boulder County’s interest in the Road of Remembrance likewise began to wane. At the insistence of the United States Bureau of Roads, the state highway department had announced in October 1927 that the State, rather than the County, would maintain the Lincoln Highway across

the County, as well as Arapahoe Road to Boulder.<sup>106</sup> The latter in fact was designated as part of Colorado State Highway 7, running from Estes Park to Lafayette.<sup>107</sup>

Though the plaque on the north pillar stated that it was the “Road of Remembrance Gateway,” the Road remained merely a dream. Apart from the Monument and adjacent park, none of the proposed amenities for the Road were ever constructed. Nor has Arapahoe Road ever been known as the “Road of Remembrance.” While the onset of the Depression and attendant decline in tourism likely were contributing factors, Post 10’s and Boulder’s concept of creating a grand boulevard into the city never materialized.

### **Legion Park**

While never directly associated with the “Road of Remembrance,” Post 10 and the County also collaborated on another memorial project on Arapahoe Road east of the city. In 1917, long before the “Road of Remembrance” project was conceived, a local Boulder businessman, John Howard Empson, deeded 26 acres to Boulder County, including Goodview Hill on the north side of Arapahoe Road overlooking the Boulder Valley.<sup>108</sup> In 1931, Boulder County commissioners requested the construction of a new scenic road to the top of Goodview Hill to provide a vantage point from which visitors will get an “unparalleled panoramic view.”<sup>109</sup>

At that point, Post 10 proposed leasing the property “for the purpose of beautifying said tract as a memorial to the soldier dead of the World War.”<sup>110</sup> In 1932, the Post’s trustees signed another lease with the County, this time for the Goodview Hill property.<sup>111</sup>

The new Legion Park was designed by Saco Rienk DeBoer, a noted landscape architect from Denver. The design for the memorial called for 2’ high rock wall, 382 feet long, and 100 feet wide with flagpoles located at each end. Two more of the captured artillery pieces would be installed on platforms near the flagpoles. Notably, two elaborate entrance gateways were planned for the entrance road off Arapahoe Road.<sup>112</sup>

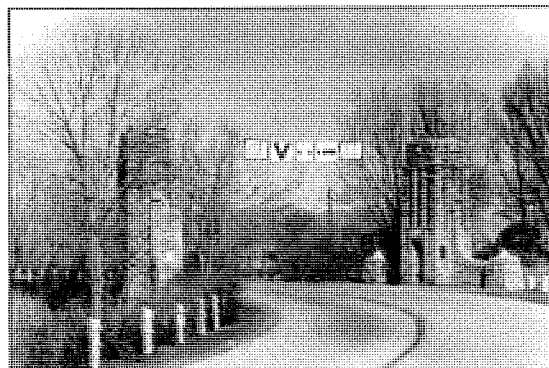
In January 1934, a crew from the Civilian Conservation Corps began construction. The memorial park with the commemorative cannons was completed, but the entrance gateways were never constructed. Legion Park was dedicated in late June 1934, and maintenance responsibilities were turned over to Legion Post 10. However, a later study concluded: “It is unclear what work, if any, occurred on the property after the American Legion signed the lease since the country was fast approaching the lowest point of the Great Depression.”<sup>113</sup>

### **The Withering of the Gateway**

Though the Road of Remembrance faded away, the Gateway remained a notable landmark in east Boulder County. For over two decades, the route east on Arapahoe Road and then south on the highway<sup>114</sup> was the primary access between Boulder and Denver. Boulder residents routinely passed between the Monument’s pillars on their way to and from the capital city.

While no records have been located concerning any maintenance on the Monument during the 1930s or 1940s, it was not forgotten by folks from Boulder. For example, the monument often was adorned with decorations during the Holiday season.<sup>115</sup> In 1942, the Boulder Chamber of Commerce purchased a new flag for the park, and arranged for Herring to care for it.<sup>116</sup>

During WWII, the Monument's patriotic heritage became more prominent. The pillars were used to promote the sale of war bonds, and display other patriotic materials to passers-by. When the County was awarded a flag in recognition of its great response to the Second War Bond Drive, the Governor suggested that rather than be displayed on Flagstaff Mountain, it fly at Nine Mile Corner due to the proximity to Longmont, Lafayette, Louisville, and Boulder.<sup>117</sup>

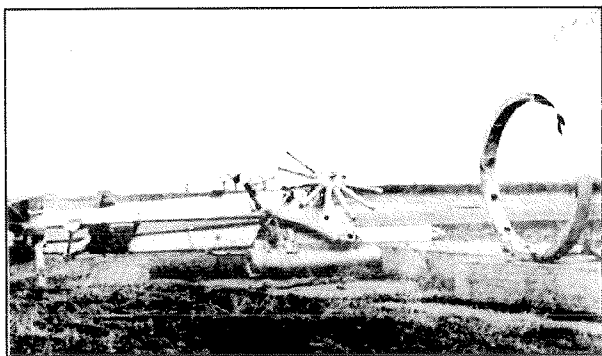


*Nine Mile Corner circa 1944*<sup>118</sup>

When WWII ended, Boulder's focus began to turn away from Nine Mile Corner. In January 1952, a four-lane toll road known as the Denver-Boulder Turnpike opened, connecting the two cities with a high speed, limited access expressway unlike anything in the state. The intercity route that began in 1928 as the Arapahoe Road – Lincoln Highway connection quickly slipped into irrelevancy for Boulderites. Tourists headed from Denver to Estes Park likewise no longer saw any need to pass through the Gateway to Boulder. The Gateway was no longer a “gateway,” but simply a relic of an earlier time.

Along with the Turnpike, the post-war population explosion in Boulder County began to impact the Gateway. In the mid-1960s, construction began on the Stonehenge subdivision in unincorporated Boulder County on Spishakoff's former land immediately northwest of the Monument.<sup>119</sup> It was soon followed by Arapahoe Ridge, an even larger development on the northeast corner of the intersection that was annexed by the Town of Erie. On the south and west, annexations by Lafayette led to developments that crept closer and closer to Nine Mile Corner. Though it remained in the unincorporated County, the Gateway no longer was out on the prairie, miles from any town.

During this time, the park at the Gateway was falling into disrepair. No record has been located suggesting that Post 10 undertook any maintenance or repair at Nine Mile Corner. Jim Hutchison, a Lafayette historian, observed that over time the cannon and flagpole were “hammered into semiruin by cars of miscalculating motorists.”<sup>120</sup>



*Cannon at Gateway Park, February 19, 1972*<sup>121</sup>

On February 19, 1972, a speeding car struck and badly damaged the cannon. The Legion indicated that the gun would be placed on it axles at the Legion Home in Boulder as soon as the Post obtained equipment to move it. State highway officials indicated that they once wanted the gun removed, but now relocation would not be required until CO 7 (Arapahoe Road) was improved in another six to eight years.<sup>122</sup> Apparently, the gun was removed at that time, but the concrete platform remained.

At the same time, Legion Park was falling into significant disrepair. In early 1976, due to the county's concern of the ability of Post 10 to properly maintain the park, Boulder County Parks and Open Space Department took control of the park's maintenance, though the Legion continued its sponsorship of the memorial. Toward that end, the County signed a Memorandum of Agreement with the Legion re-executing their March 1, 1933 lease, and outlining that the County would take responsibility for the care of the park.<sup>123</sup>

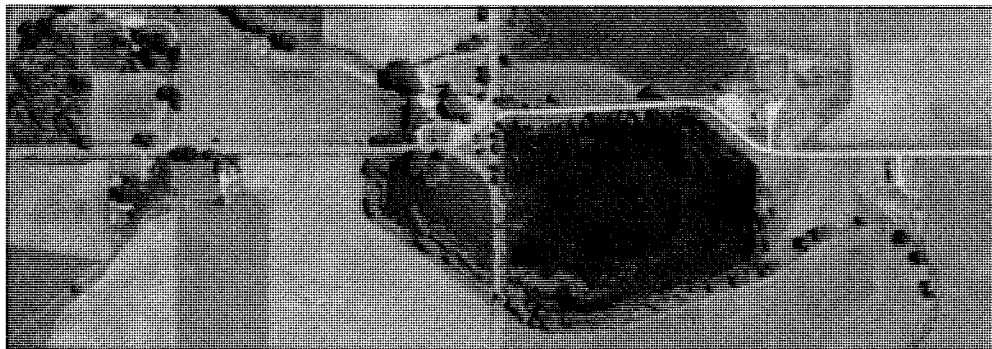
Nothing in that document mentioned the Monument at Nine Mile Corner. Nor has any legal instrument regarding control of the Monument, similar to the leases for the two parks, been located. Likewise, no record has been found reflecting any maintenance of the Monument by the County, the State, the Legion, or any other party from 1928 until 1983.

In May 1980, the Monument was part of a Colorado Cultural Resource Survey prepared by the Boulder County Historical Society. The Monument's owner was listed as "The Lions Club of Boulder." The Survey indicated that the Monument was on its original site, had not been altered and was in good condition, though the stone "may need to be repointed in the near future."<sup>124</sup> The Monument was not shown as having any architectural significance, but was listed as having historical significance as "associated with significant events or patterns."<sup>125</sup> Accordingly, the Monument was considered "field eligible" to be listed on the National Register of Historic Places, though no official determination has been made by the State Historic Preservation Office.<sup>126</sup>

### **The Widening of Arapahoe Road in 1983**

When the Gateway park was completed in 1929, Boulder County's population was roughly 32,000. By 1980, the number of residents had grown to nearly 190,000, nearly a six-fold increase. Though Nine Mile Corner had ceased to be part of the main thoroughfare between Boulder and Denver, it was used as a local artery by the increasing numbers of suburbanites in the East County. The time for "modernizing" the intersection had come.

As reflected above (see map, p. 6), Arapahoe Road east of the new Lincoln Highway veered slightly northward to avoid the Prince Reservoir. When he designed the "wye," Douglas oriented the northern arc to align with the continuation of Arapahoe Road east of Nine Mile Corner.



*Aerial view of Nine Mile Corner in 1949*<sup>127</sup>

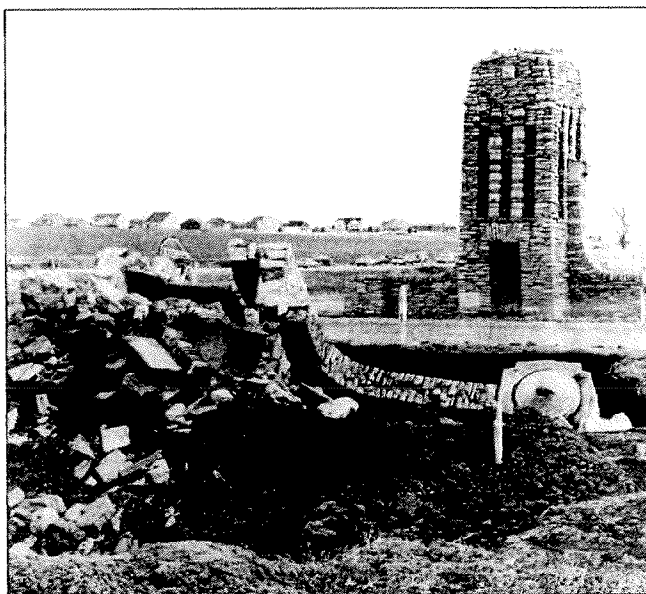
In the early 1980s, a decision was made to change the configuration of the intersection, *inter alia*, by shifting Arapahoe Road on the east side of the intersection to the south to more

directly align with Arapahoe Road (CO 7) on the west side, and widening Arapahoe Road west of the intersection. The project was identified by CDOT as Project No. FC-HES 007-2. In preparation for that project, the Town of Eric executed a Warranty Deed in February 1983 transferring to the State several small parcels to comprise part of the expanded right of way for Arapahoe Road on the west side of US 287.<sup>128</sup> Additional land for the right of way was acquired by the State from other landowners.<sup>129</sup>

As part of the project, the Gateway was substantially modified by (a) removing and paving over the remaining vestiges of the triangular park, and (b) moving the north pillar of the Monument further north to allow for the widening of Arapahoe Road.<sup>130</sup> No record has been located reflecting the deliberations underlying that decision. Research indicates that a study (not yet located) was prepared for the project by the Colorado Department of Highways entitled "Project Fc-Hes 007-2(s), S.H. 7 and U.S. 287; Cultural Resource Report for Historical Resources,"<sup>131</sup> which presumably discusses those issues.

Flatiron Paving Company was hired by the State to perform the work on Arapahoe Road in 1983. During that construction work, the remnants of the Gateway park were demolished by Flatiron to make way for the widened road. With regard to the Monument, Flatiron in April 1983 subcontracted with Whitlock House Moving of Louisville to move the north pillar 40 feet further north.<sup>132</sup>

Prior to moving the Monument, a "methods statement" reportedly was submitted to the state highway department and the State Historical Society, though that document has not been located. According to James Combs from the state highway department, the plan involved digging into the sides of the north pillar, jacking it up, putting steel beams beneath it, and then sliding it to the new location.<sup>133</sup>



*North Pillar, April 1983* <sup>134</sup>

A witness described what occurred when Whitlock attempted to move the pillar: "This old, wet sandy soil just gave way. It took about 10 minutes for it to fall over. It leaned over a little at a time, and the bank was caving in. Then when it hit, it crumbled to pieces. It's too bad."<sup>135</sup>

Flatiron agreed to rebuild the Monument, with a spokesman commenting: "We've got to construct the monument back to what it originally looked like. It's hard to tell at this point what the cost'll be. I guess if it wasn't a historical monument we could get by a lot cheaper."<sup>136</sup>

The reconstruction plan involved cleaning the unbroken flagstone, and replacing the broken stones with flagstone of matching color. Since Walter's original written plans were not available, the rebuilt pillar was modelled after the still-standing south pillar, using similar techniques as the original.<sup>137</sup>

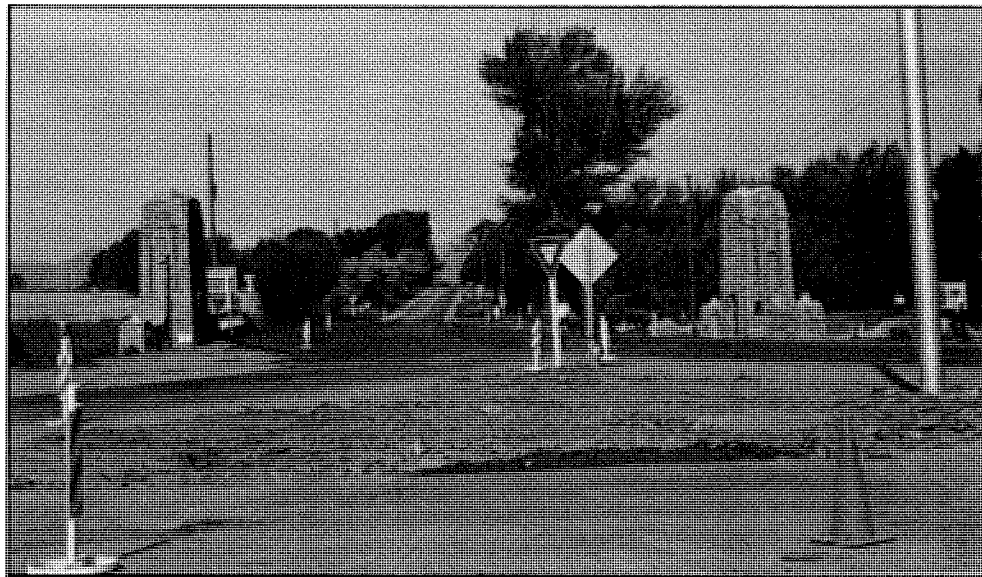
Flatiron Paving hired Del's Masonry of Longmont to rebuild the pillar. Del's assigned Lorri and Roy Martinez to do the work. The new pillar was rebuilt approximately 40' north of the location of the original north pillar. The Martinezes completed the work in the summer 1983, using about 80% of the original stones.<sup>138</sup> The restoration work took several weeks, and cost roughly \$20,000.<sup>139</sup>



*Rebuilding the north pillar, July 1983*<sup>140</sup>

Apart from press reports, no documentation from the 1983 project regarding the Monument has been located. Nor has any survey data been discovered that gives the precise location of the rebuilt north pillar. Based on the best available information, it appears that the north pillar was rebuilt – and currently remains – on land acquired for the widening project by the State in 1983 from a private landowner.<sup>141</sup>

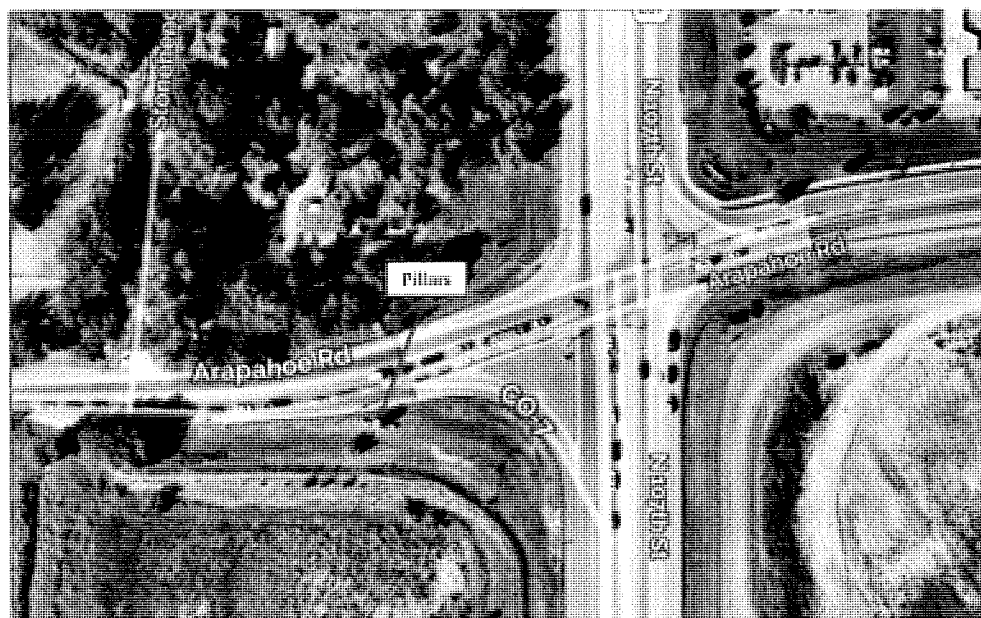
After the north pillar was rebuilt, the spacing between the pillars was significantly increased to allow for the additional traffic lanes.



*1983 Widening and Reconstruction*<sup>142</sup>

With the completion of the 1983 reconstruction of the intersection, the Gateway as planned and constructed in 1928 had been largely demolished. The two sweeping arcs of the roadway joining at the Monument were replaced by a significantly widened single ribbon of concrete. The

park, cannon and flagpole were gone, covered over with hardscape. While it appears that the south pillar remained in its original location, the north pillar had been destroyed and rebuilt 40' from its original location.



*Nine Mile Corner 2020*

### **The 2021 Reconstruction of the Intersection**

At this time, no documentation has been located to suggest that any significant additional construction occurred near the Monument between 1983 and 2021. However, in 2021, a new project again impacted the remaining portions of the Monument.

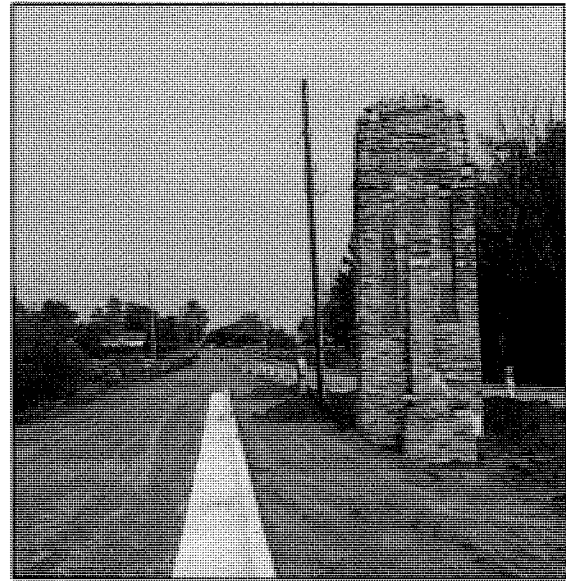
The project, known as the Nine Mile Corner Development, lay on the southeast corner of the intersection. In 2015, the Town of Erie announced a public-private partnership with Evergreen Devco, Inc. to develop the 45-acre tract. As part of the development, it was decided to add lanes to Arapahoe Road and reconfigure its intersection with US 287.

Those planning the new development were aware of the potential impacts of the project on the Monument. On October 20, 2015, Tyler Carlson, a principal at Devco, publicly stated that the construction would “complement the existing historical architecture” and “honor the 1928 vision of beautifying the intersection where the historic Road of Remembrance Gateway still stands.”<sup>143</sup>

For several years, the development was delayed by litigation between Erie and Lafayette. These disputes eventually were resolved, and Evergreen retained Galloway & Company, Inc. to design the improvements to the intersection, including on the west side on Arapahoe Road. According to Galloway, it “collaborated closely with” the Erie, Lafayette, the Colorado Department of Transportation (CDOT), and Boulder County in developing its plan.<sup>144</sup> Because Arapahoe Road west of the intersection is a state highway, the primary jurisdiction for approval of the design and completion of the work in the vicinity of the Monument lay with the CDOT.

The plan prepared by Galloway involved adding another eastbound through traffic lane on the west side of the intersection. To make room for that lane, the eastbound right turn lane was relocated, so that it passed south of the south pillar of the Monument.

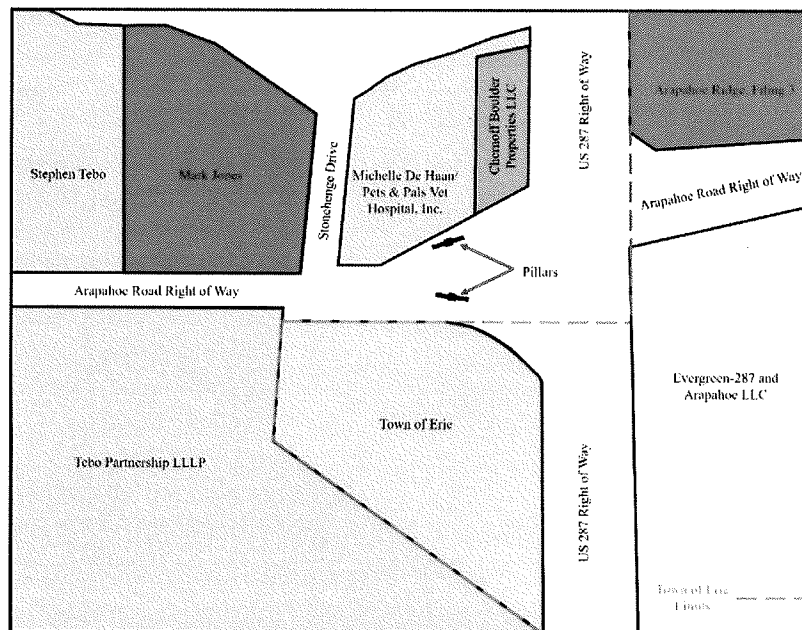
During this construction, the south pillar was not physically altered. Instead, it was encircled by a raised “bump out” traffic island covered with hardscape, and protected by guard rails. The north pillar of the Monument was untouched by this project, and remains where it was rebuilt on the north shoulder of Arapahoe Road/CO 7 in 1983. Both are in significant disrepair with crumbling masonry, deteriorating concrete ornamentation, and broken windows.



*Intersection on August 1, 2021*

### **The Future**

Development in the area is continuing, and perhaps accelerating. At this writing, Lafayette has annexed and approved a new urban mixed-use community with 473 dwelling units plus nearly 30,000 ft<sup>2</sup> of office/commercial space, which is under construction on Arapahoe Road a few hundred feet west of the Monument.<sup>145</sup> Additional developments are in the planning stages immediately to the south and west of the Monument.



*Land Ownership 2021*<sup>146</sup>

At the same time, Arapahoe Road/State Highway 7 has become “a critical east-west arterial in the regional transportation system serving Boulder, Lafayette, Erie, unincorporated Boulder County, and the north Denver metropolitan area.”<sup>147</sup> With the ever-increasing population in the area, coupled with the prospect of future development in the vicinity, it appears likely that transportation authorities will undertake additional construction/reconfiguration of the intersection at Nine Mile Corner within the next twenty years.

William D. Meyer  
Boulder Rotary Club  
Revised September 27, 2021

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<sup>1</sup> The Lincoln Highway Association, *The Lincoln Highway: The Story of a Crusade that Made Transportation History*, p. 60, New York: Dodd & Mead, 1935; Wolfe, Mark, “How the Lincoln Highway Snubbed Colorado,” *Colorado Heritage*, p. 3, (Autumn 1999).

<sup>2</sup> See “Proclamation of route of The Lincoln Highway,” found at [https://en.wikisource.org/wiki/Proclamation\\_of\\_route\\_of\\_The\\_Lincoln\\_Highway](https://en.wikisource.org/wiki/Proclamation_of_route_of_The_Lincoln_Highway) [accessed August 8, 2021].

<sup>3</sup> *Ibid.*

<sup>4</sup> “Colorado Protesting,” *The Daily Deadwood Pioneer-Times*, March 21, 1915; *The Lincoln Highway: The Story of a Crusade that Made Transportation History*, p. 158; Wolfe, p. 3.

<sup>5</sup> See *The Complete Official Road Guide of the Lincoln Highway*, Second Edition, p. 117, Detroit: The Lincoln Highway Association, 1916 .

<sup>6</sup> Wolfe, *supra*.

<sup>7</sup> Connarroe, Doug, “For two brief years, the (official) Lincoln Highway came through Lafayette,” September 4, 2019, found at <https://www.lafayettehistory.com/the-two-brief-years-that-the-official-lincoln-highway-came-through-lafayette/> [accessed August 5, 2021]. See, e.g., “Commissioners To Over New Route of Highway Next Week,” *The Daily Times*, June 26, 1925.

<sup>8</sup> Original map titled “Road and trail map, Boulder County, Colorado,” drawn by Art Boase, circa 1920, located in the Jerry Crail Johnson Earth Sciences & Map Library, University of Colorado (“Boase Map”).

<sup>9</sup> Original from Boase Map.

<sup>10</sup> Letter dated September 27, 1919 from David H. Holmes to George P. Schumaker, found in “Colorado Roads Scrapbook 1916-1920,” Call No. BHS 300-2-11, maintained by the Carnegie Library for Local History, Boulder, Colorado.

<sup>11</sup> “Roads from Lyons to Boulder and Longmont to be paved,” *The Lyons Recorder*, Vol. XIX, No. 31 (July 10, 1920); “Road Enthusiasts Outline Campaign,” *Fort Collins Coloradan*, December 10, 1920; letters dated November 26, 1920 from Harry E. Niven to David H. Holmes, and January 14, 1921 from David H. Holmes to Wm. G. Evans, found in “Colorado Roads Scrapbook 1916-1920,” Call #BHS 300-2-11, maintained by the Carnegie Library for Local History, Boulder, Colorado.

<sup>12</sup> “Charles L. Pack Suggests Highways Instead of Stone Monuments,” *Washington Post*, p. 30, June 15, 1919; “Memorial Trees,” *American Forestry*, Vol XXV, No. 307, p. 1201 (July 1919); *id.*, “For Them a Tree Stands,” p. 1270.

<sup>13</sup> “Editors Take Up Forest Matters,” *American Forestry*, Vol XXV, No. 307, p. 1296 (July 1919).

<sup>14</sup> “Roads of Remembrance,” *American Forestry*, Vol XXV, No. 309, p. 1334 (September 1919).

<sup>15</sup> See Wheat, George Seay, *The Story of the American Legion*, New York: G. P. Putnam’s Sons, 1919.

<sup>16</sup> “Legion to Plant Trees,” *Los Angeles Herald*, Vol. XLVI, No. 281 (September 24, 1921).

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<sup>17</sup> *Lyons Recorder*, Vol. XIX, No. 19 (October 18, 1919).

<sup>18</sup> "Memory Trees to Soldiers," *Longmont Ledger* (July 1, 1921).

<sup>19</sup> Clipping dated September 7, 1927 titled "Chauncey Vivian Tells the World of Boulder's Remembrance Road," ("Vivian Article") located in the Boulder Daily Camera Archives maintained by the Carnegie Library for Local History, Boulder, Colorado. Vivian was the commander of Boulder Legion Post 10 in the 1920s.

<sup>20</sup> "Lions Outlined Plans to Help Clubs of City and Chamber in Their Co-Operative Efforts," *Boulder Daily Camera*, January 17, 1923, located in the Boulder Daily Camera Archives maintained by the Carnegie Library for Local History, Boulder, Colorado.

<sup>21</sup> "County Will Improve Base Line Road," *The Lafayette Leader*, Vol. XX, No 4 (January 26, 1923).

<sup>22</sup> Vivian Article.

<sup>23</sup> Vivian Article.

<sup>24</sup> See, e.g., "Auto Overturns, Five Are Hurt," *The Longmont Call*, Vol. XVIII, No. 15 (December 17, 1915); "'Bob' Snow Somersaults In Morgan Ford Truck," *The Lafayette Leader*, Vol. XV, No 50, (December 13, 1918).

<sup>25</sup> "Lafayette People Opposed to Change in Highway Route and Threaten to Bring Injunction," *The Daily Times*, Vol. XXXI, No. 153 (June 11, 1925).

<sup>26</sup> "Tentative Plans Made for Paving," *The Lafayette Leader*, Vol. XX, No 50 (December 14, 1923).

<sup>27</sup> "Lafayette People Opposed to Change in Highway Route and Threaten to Bring Injunction," *The Daily Times*, Vol. XXXI, No. 153 (June 11, 1925).

<sup>28</sup> *Ibid.*

<sup>29</sup> *Ibid.*

<sup>30</sup> *Ibid.*

<sup>31</sup> "Farmers on Proposed Highway Route Refuse to Sell County Right-of-Way," *The Lafayette Leader*, Vol. XXII, No 30 (July 24, 1925).

<sup>32</sup> "That Break between Longmont & Boulder," *The Daily Times*, Vol. XXXI, No. 163 (June 23, 1925).

<sup>33</sup> *Ibid.*

<sup>34</sup> "Commissioners Go Over New Route of Highway Next Week," *The Daily Times*, Vol. XXXI, No. 166 (June 26, 1925).

<sup>35</sup> *Ibid.*; "Farmers Signing Up for New Right of Way South of Longmont," *The Daily Times*, Vol. XXXI, No. 174 (July 7, 1925).

<sup>36</sup> "Farmers Signing Up for New Right of Way South of Longmont," *The Daily Times*, Vol. XXXI, No. 174 (July 7, 1925).

<sup>37</sup> "State Highway Engineer Decides on West Route for New Paved Road," *The Lafayette Leader*, Vol. XXII, No 37 (September 11, 1925).

<sup>38</sup> "Farmers on Proposed Highway Route Refuse to Sell County Right-of-Way," *The Lafayette Leader*, Vol. XXII, No 30 (July 24, 1925).

<sup>39</sup> "State Highway Engineer Decides on West Route for New Paved Road," *The Lafayette Leader*, Vol. XXII, No 37 (September 11, 1925).

<sup>40</sup> "State Files Suit to Build New Highway," *The Daily Times*, Vol. XXXI, No. 247 (October 3, 1925).

<sup>41</sup> "State to Pave Longmont Road if Lafayette Stretch is OK," *The Daily Times*, Vol. XXXI, No. 276 (November 2, 1925).

<sup>42</sup> "Jury Gives Verdict in Highway Case," *The Daily Times*, Vol. XXXII, No. 8 (December 24, 1925).

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<sup>43</sup> “Right-of-Way Question is Expected to be Paramount One at Meeting of Advisory Board of State Next Week,” *The Daily Times*, Vol. XXXII, No. 25 (January 15, 1926).

<sup>44</sup> “Farmers to Dismiss Road Case Monday Boulder Newspaper Says,” *The Daily Times*, Vol. XXXII, No. 38 (January 3, 1926).

<sup>45</sup> Vivian Article.

<sup>46</sup> *Ibid.*

<sup>47</sup> “County Will Improve Base Line Road,” *The Lafayette Leader*, Vol. XX, No 4 (January 26, 1923).

<sup>48</sup> “Booty from the Boche,” *The American Legion Weekly*, Vol. 4, p. 11 (September 8, 1922).

<sup>49</sup> “Parade to Escort Trophies for Lodge on Lookout,” *Jefferson County Republican*, Vol. 6, No. 36 (July 30, 1925).

<sup>50</sup> Legion Park Landmark Nomination Form (undated), prepared by Boulder County Land Use Department, attached to Agenda for Boulder County Historic Preservation Advisory Board, September 5, 2019 (“Legion Park Nomination Form”).

<sup>51</sup> In the early years, the spot was also known as “the new Ten Mile Corner” or “Four Mile Corner,” the latter presumably based on the distance from Lafayette.

<sup>52</sup> Boase Map.

<sup>53</sup> “Plans for New Road Submitted by Local Engineer to State,” *The Daily Times*, Vol. XXXII, No. 65 (March 3, 1926).

<sup>54</sup> “Plans Nearing Completion for Right of Way Highway; Local Engineer Working on New Route,” *The Daily Times*, Vol. XXXII, No. 60 (February 25, 1926).

<sup>55</sup> Vivian Article.

<sup>56</sup> See Weingroff, Richard F., “From Names to Numbers: The Origins of the U.S. Numbered Highway System,” Federal Highway Administration (June 27, 2017), found at <https://www.fhwa.dot.gov/infrastructure/numbers.cfm> [accessed August 17, 2021].

<sup>57</sup> “World’s Greatest Road May Be Built Through Longmont,” *The Daily Times*, Vol. XXXII, No. 58 (February 23, 1926).

<sup>58</sup> “Plans Nearing Completion for Right of Way Highway; Local Engineer Working on New Route,” *The Daily Times*, Vol. XXXII, No. 60 (February 25, 1926).

<sup>59</sup> “Right-of-Way Damages Expected by Commissioners in Next Week,” *The Daily Times*, Vol. XXXII, No. 44 (February 6, 1926). See also “Governor May Ask Supreme Court Today to Untangle Hiway Mixup,” *The Daily Times*, Vol. XXXII, No. 30 (January 21, 1926).

<sup>60</sup> “One Million Autos Annually Use Lincoln Highway,” *The Daily Times*, Vol. XXXII, No. 35 (September 27, 1926).

<sup>61</sup> “‘Right This Way to Estes Park,’ Cries Boulder Man Stationed in Hut at 10-mile Road Intersection,” *The Daily Times*, Vol. XXXII, No. 172 (July 8, 1926); clipping titled “Longmont Editor Objects To Plan Of Boulder To Get Some Tourists,” *Boulder Daily Camera* (July 9, 1926), located in the Boulder Daily Camera Archives maintained by the Carnegie Library for Local History, Boulder, Colorado.

<sup>62</sup> “Boulder ‘Steering’ Hut Burns Friday Night,” *The Lafayette Leader*, Vol. XIV, No. 30 (July 23, 1926); clipping titled “Legion Information Booth at Ten-Mile Corner Mysteriously Burned to Ground Last Night; Incendiarism Is Suspected,” *Boulder Daily Camera* (July 17, 1926), located in the Boulder Daily Camera Archives maintained by the Carnegie Library for Local History, Boulder, Colorado.

<sup>63</sup> “Boulder ‘Steering’ Hut Burns Friday Night,” *The Lafayette Leader*, Vol. XIV, No. 30 (July 23, 1926).

<sup>64</sup> “Boulder Tourist Hut at Ten Mile Corner Burned,” *The Daily Times*, Vol. XXXII, No. 180 (July 17, 1926). See also “Water Flow in River,” *The Daily Times*, Vol. XXXI, No. 159 (June 18, 1925).

<sup>65</sup> “‘Right This Way to Estes Park,’ Cries Boulder Man Stationed in Hut at 10-mile Road Intersection,” *The Daily Times*, Vol. XXXII, No. 172 (July 8, 1926); “Boulder Paper Sees Unfairness of ‘Man in Hut’ at Intersection,” *The*

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*Daily Times*, Vol. XXXII, No. 179 (July 16, 1926); "Boulder 'Steering' Hut Burns Friday Night," *The Lafayette Leader*, Vol. XIV, No. 30 (July 23, 1926).

<sup>66</sup> "New Longmont Road Sign Will Properly Direct All Tourists to Destination," *The Daily Times*, Vol. XXXII, No. 174 (July 10, 1926).

<sup>67</sup> See Connarroe, Doug, "The battle over Ten Mile Corner and how Nine Mile Corner in Erie got its name," September 11, 2019, found at <https://www.lafayettehistory.com/how-nine-mile-corner-in-erie-got-its-name/> [accessed June 21, 2021].

<sup>68</sup> "Boulder Tourist Hut at Ten Mile Corner Burned," *The Daily Times*, Vol. XXXII, No. 180 (July 17, 1926); "Comments: The Tourist Bureau at Ten-Mile Corner," *The Daily Times*, Vol. XXXII, No. 181 (July 19, 1926); clipping titled "Legion Information Booth at Ten-Mile Corner Mysteriously Burned to Ground Last Night; Incendiarism Is Suspected," *Boulder Daily Camera* (July 17, 1926), located in the Boulder Daily Camera Archives maintained by the Carnegie Library for Local History, Boulder, Colorado.

<sup>69</sup> "Boulder 'Steering' Hut Burns Friday Night," *The Lafayette Leader*, Vol. XIV, No. 30 (July 23, 1926).

<sup>70</sup> "Comments: The Tourist Bureau at Ten-Mile Corner," *Daily Times*, Vol. XXXII, No. 181 (July 19, 1926); "Boulder 'Steering' Hut Burns Friday Night," *The Lafayette Leader*, Vol. XIV, No. 30 (July 23, 1926).

<sup>71</sup> See "Boulder-Longmont Joint Program Armistice Day Will Not Be held," *The Daily Times*, Vol. XXXII, No. 240 (September 25, 1926).

<sup>72</sup> "Longmont-Lafayette Road Will Be Completed As Budget Is Accepted," *The Daily Times*, Vol. XXXIII, No. 21 (January 7, 1927).

<sup>73</sup> "Three Paving Districts Will Connect Links Thru Lafayette North," *The Daily Times*, Vol. XXXII, No. 111 (April 26, 1926). See also Lease recorded December 30, 1927 at Book 559, p. 139 in the records of the Boulder County Clerk and Recorder.

<sup>74</sup> Judgment and Decree recorded on February 11, 1926 at Book 530, p. 372 in the records of the Boulder County Clerk and Recorder.

<sup>75</sup> Deed recorded November 26, 1926 at Book 543, p. 519 in the records of the Boulder County Clerk and Recorder.

<sup>76</sup> Deed recorded May 26, 1926 at Book 543, p. 52 in the records of the Boulder County Clerk and Recorder.

<sup>77</sup> "Grading and Drainage Bids Will Be Opened June 18," *The Lafayette Leader*, Vol. XIV, No. 24 (June 11, 1926).

<sup>78</sup> "Pouring of Concrete on 6-Mile Stretch South of City Started by Miller," *The Daily Times*, Vol. XXXIII, No. 243 (September 24, 1927).

<sup>79</sup> "Plans for New Road Submitted by Local Engineer to State," *The Daily Times*, Vol. XXXII, No. 65 (March 3, 1926).

<sup>80</sup> Vivian Article.

<sup>81</sup> *Ibid.*

<sup>82</sup> Architectural/Historical Component Form for Resource No. 5 BL 432 dated May 20, 1980, obtained from the records of the Boulder County Planning Department.

<sup>83</sup> Clipping dated April 11, 1928, titled "Boulder Lions Club to Lay Cornerstone for New Gate Wednesday," located in folder 328-52-20 maintained by the Carnegie Library for Local History, Boulder, Colorado.

<sup>84</sup> *Ibid.*

<sup>85</sup> Undated clipping titled "Lions Club Breaks Ground for Memorial Gateway East of City," located in folder 328-52-20 maintained by the Carnegie Library for Local History, Boulder, Colorado.

<sup>86</sup> Clipping titled "Lions Club Paid its Respects to Those Who Served in War," *The Boulder Daily Camera*, June 18, 1928, maintained by the Carnegie Library for Local History, Boulder, Colorado.

<sup>87</sup> Clippings titled "Boulder Lions Club To Lay Cornerstone For New Gate Wednesday," dated April 11, 1928; "Cornerstone for Boulder Lions Memorial Gateway Will Be Laid In Impressive Services," dated April 17, 1928;

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"Lions Club Breaks Ground for Memorial Gateway East of City," dated April 18, 1928; "Speakers Dedicate Lions Club Gateway to Memory of Boulder World War Dead," dated April 18, 1928, located in folder 328-52-20 maintained by the Carnegie Library for Local History, Boulder, Colorado.

<sup>88</sup> Information from Gail Elias, Regent, Indian Peaks Chapter, NSDAR.

<sup>89</sup> Clipping titled "Lions Club Paid its Respects to Those Who Served in War," *The Boulder Daily Camera*, June 18, 1928, maintained by the Carnegie Library for Local History, Boulder, Colorado; "Boulder Gate Is Dedicated," *Fort Collins Express-Courier*, (June 18, 1928). See also undated pamphlet titled "Greetings from the Lions in the Den," located in the Lions Club Archives maintained by the Carnegie Library for Local History, Boulder, Colorado.

<sup>90</sup> Vivian Article.

<sup>91</sup> Lease recorded December 30, 1927 at Book 559, p. 139 in the records of the Boulder County Clerk and Recorder.

<sup>92</sup> Clipping titled "Lions Club Paid its Respects to Those Who Served in War," *The Boulder Daily Camera*, June 18, 1928, maintained by the Carnegie Library for Local History, Boulder, Colorado.

<sup>93</sup> In press reports, there was confusion over which of the guns was installed at Nine Mile Corner. When that gun was destroyed in 1972, one press report referred to it as a 75 mm gun. Clipping dated March 13, 1972 from the *Longmont Times-Call* titled "Direct Hit Means Removal," provided by the Longmont Museum. However, other reports and photographs confirm that the gun at Nine Mile Corner was the 100 mm gun received by Post 10. The captured 77 mm gun was initially installed at Legion Park, and later restored and moved to Post 10's facility on North Broadway in Boulder. See Legion Park Nomination Form.

<sup>94</sup> Clipping dated July 25, 1990, titled "Pillars are war memorial, landmark in east county," located in the Boulder Daily Camera Archives maintained by the Carnegie Library for Local History, Boulder, Colorado. See also "Our Neighbors," *The Lafayette Leader*, Vol. XV, No. 15 (May 24, 1929), and "Local Items," *The Lafayette Leader*, Vol. XV, No. 17 (June 7, 1929).

<sup>95</sup> Legion Park Nomination Form.

<sup>96</sup> Undated photo from "A.A. Paddock Collection: East Boulder County," maintained by the Carnegie Library for Local History, Boulder, Colorado.

<sup>97</sup> "Unable to Prevent Placing Billboards," *The Daily Times*, Vol. XXXIII, No. 235 (September 25, 1927).

<sup>98</sup> "Offers the Olive Branch," *Fort Collins Express-Courier*, (April 11, 1928).

<sup>99</sup> Undated photo from "A.A. Paddock Collection: East Boulder County," maintained by the Carnegie Library for Local History, Boulder, Colorado.

<sup>100</sup> *Ibid.*

<sup>101</sup> Deed dated August 18, 1928 at Book 553, p. 464 in the records of the Boulder County Clerk and Recorder.

<sup>102</sup> Deeds dated May 19, 1930 recorded at Book 581, p. 380 and September 15, 1931 recorded at Book 582, p. 555 in the records of the Boulder County Clerk and Recorder. See also "New Flag for Nine Mile Corner," *The Lafayette Leader*, Vol. 38, No. 21 (April 17, 1943).

<sup>103</sup> "Boulder County Items," *The Lafayette Leader*, Vol. XVI, No. 31 (August 3, 1928).

<sup>104</sup> Undated photo from "A.A. Paddock Collection: East Boulder County," maintained by the Carnegie Library for Local History, Boulder, Colorado.

<sup>105</sup> "Boulder County Items," *The Lafayette Leader*, Vol. XVI, No. 27 (July 6, 1928).

<sup>106</sup> "Federal Road Bureau Desires to Maintain State Highways," *Daily Times*, Vol. XXXIII, No. 254 (October 7, 1927).

<sup>107</sup> See Deed dated August 18, 1928 at Book 553, p. 464 in the records of the Boulder County Clerk and Recorder.

<sup>108</sup> Deed dated November 26, 1917 at Book 416, p. 278 in the records of the Boulder County Clerk and Recorder.

<sup>109</sup> Legion Park Nomination Form.

<sup>110</sup> Lease recorded March 20, 1933 at Book 612, p. 170 in the records of the Boulder County Clerk and Recorder.

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<sup>111</sup> *Ibid.*

<sup>112</sup> Legion Park Nomination Form.

<sup>113</sup> *Ibid.*

<sup>114</sup> In 1925, the United States Secretary of Agriculture approved a controversial plan to implement a system of marked and numbered “interstate highways” on major roads receiving federal funding. These numbered routes were intended to supersede and simplify the burgeoning numbers of privately sponsored “auto trails” such as the Lincoln Highway. See Weingroff, *supra*. Records indicate that the route from Lafayette to Longmont was initially included as part of US 85, later renumbered as US 285, then US 87, and finally the current US 287. At the same time, at least in the early years, the road was also referred to as Colorado State Highway 1. See Deed dated August 18, 1928 at Book 553, p. 464 in the records of the Boulder County Clerk and Recorder.

<sup>115</sup> Undated photo from “A.A. Paddock Collection: East Boulder County,” maintained by the Carnegie Library for Local History, Boulder, Colorado.

<sup>116</sup> “New Flag for Nine Mile Corner,” *The Lafayette Leader*, Vol. 39, No. 21 (April 17, 1943).

<sup>117</sup> “Bond Flag to Fly at Nine Mile Corner,” *The Lafayette Leader*, Vol. 39, No. 23 (April 30, 1943).

<sup>118</sup> Undated photo from “A.A. Paddock Collection: East Boulder County,” maintained by the Carnegie Library for Local History, Boulder, Colorado.

<sup>119</sup> Plat recorded June 24, 1963 at Plat Book 9, p. 39 in the records of the Boulder County Clerk and Recorder.

<sup>120</sup> Clipping dated July 25, 1990, titled “Pillars are war memorial, landmark in east county,” located in the Boulder Daily Camera Archives maintained by the Carnegie Library for Local History, Boulder, Colorado.

<sup>121</sup> *The Lafayette Leader*, Vol. 71, No. 4 (February 24, 1972).

<sup>122</sup> *Ibid*; clipping dated March 13, 1972 from the *Longmont Times-Call* titled “Direct Hit Means Removal,” provided by the Longmont Museum; clipping dated July 25, 1990, titled “Pillars are war memorial, landmark in east county,” located in the Boulder Daily Camera Archives maintained by the Carnegie Library for Local History, Boulder, Colorado.

<sup>123</sup> Legion Park Nomination Form.

<sup>124</sup> Inventory Record submitted with Architectural/Historical Component Form for Resource No. 5 BL 432 dated May 12, 1980, obtained from the records of the Boulder County Planning Department.

<sup>125</sup> Architectural/Historical Component Form for Resource No. 5 BL 432 dated May 20, 1980, obtained from the records of the Boulder County Planning Department.

<sup>126</sup> Inventory Record submitted with Architectural/Historical Component Form for Resource No. 5 BL 432 dated May 12, 1980, obtained from the records of the Boulder County Planning Department; information received from CDOT Region 4.

<sup>127</sup> USGS Aerial Photograph, Project GS ID taken July 14, 1949, from the Arthur Lakes Library at the Colorado School of Mines.

<sup>128</sup> Warranty Deed recorded February 1, 1983 at Reception No. 532304 in the records of the Boulder County Clerk and Recorder.

<sup>129</sup> Warranty Deeds recorded March 7, 1983 at Reception Nos. 536538 and 536539, and Order recorded July 30, 1984 at Reception No. 666149, in the records of the Boulder County Clerk and Recorder.

<sup>130</sup> No record has been located indicating that the south pillar was moved or modified. The 1980 Colorado Cultural Resource Survey form prepared by the Boulder County Historical Society (above) states that “mementos of the dedication” are kept inside the metal doors of the pillars. However, the reports from 1928 indicate that those mementos were sealed in the cornerstone of the south pillar. If the comment in the Survey is accurate, then it appears that the cornerstone was removed at some point. Whether, when and how that occurred is unknown.

<sup>131</sup> See <https://core.tdar.org/document/156704/project-fc-hes-007-2s-sh-7-and-us-287-cultural-resource-report-for-historical-resources> [accessed August 1, 2021].

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<sup>132</sup> Clipping dated April 2, 1983 titled “Monument topples,” located in the Boulder Daily Camera Archives maintained by the Carnegie Library for Local History, Boulder, Colorado.

<sup>133</sup> “Monument at Highway 7-287 comes tumbling down,” *The Louisville Times*, Vol. 69, No. 44 (April 20, 1983).

<sup>134</sup> *Louisville Times*, Vol. 69, No. 44 (April 20, 1983).

<sup>135</sup> “Monument at Highway 7-287 comes tumbling down,” *The Louisville Times*, Vol. 69, No. 44 (April 20, 1983).

<sup>136</sup> “Monument at Highway 7-287 comes tumbling down,” *The Louisville Times*, Vol. 69, No. 44 (April 20, 1983); clipping dated April 2, 1983 titled “Monument topples,” located in the Boulder Daily Camera Archives maintained by the Carnegie Library for Local History, Boulder, Colorado.

<sup>137</sup> “Monument to be rebuilt,” *The Louisville Times*, Vol. 69, No. 47 (May 11, 1983).

<sup>138</sup> Clipping dated July 9, 1983 titled “Monument going back up,” located in the Boulder Daily Camera Archives maintained by the Carnegie Library for Local History, Boulder, Colorado.

<sup>139</sup> Uncaptioned photo, *The Louisville Times*, Vol. 70, No. 3 (July 6, 1983).

<sup>140</sup> *The Louisville Times*, Vol. 70, No. 3 (July 6, 1983).

<sup>141</sup> Warranty Deed recorded March 7, 1983 at Reception No. 536538 in the records of the Boulder County Clerk and Recorder.

<sup>142</sup> Photo dated August 5, 1983 from the records of the Boulder County Planning Department.

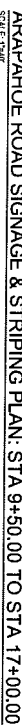
<sup>143</sup> Munio, Natalie, “87-year-old-pillars devoted to WWI vets at US-287 & Arapahoe gaining new attention,” *Colorado Hometown Weekly* (November 10, 2015) found at <https://www.coloradohometownweekly.com/2015/11/10/87-year-old-pillars-devoted-to-wwi-vets-at-us-287-arapahoe-gaining-new-attention/> [accessed July 27, 2021].

<sup>144</sup> See <https://gallowayus.com/project/nine-mile-corner/> [accessed July 27, 2021].

<sup>145</sup> See Silo Subdivision Final Planned Unit Development (PUD), approved by the Lafayette City Council, October 17, 2017.

<sup>146</sup> Data obtained from Boulder County Assessor’s Office website found at <https://www.bouldercounty.org/departments/assessor/> [accessed September 7, 2021].

<sup>147</sup> “Arapahoe Road / CO State Highway 7: Planning & Environmental Linkage (PEL) Study Update,” presented to the Boulder County Open Space Board of Trustees, dated August 9, 2017.



- (1) PROPOSED THEN MONASTIC LEFT TURN SIGNAL WHITE
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- (5) PROPOSED THEN MONASTIC A WHITE SOLID LINE WHITE
- (6) PROPOSED THEN MONASTIC A WHITE SOLID LINE WHITE
- (7) PROPOSED THEN MONASTIC A WHITE SOLID LINE WHITE
- (8) PROPOSED THEN MONASTIC A WHITE SOLID LINE WHITE
- (9) PROPOSED THEN MONASTIC A WHITE SOLID LINE WHITE
- (10) PROPOSED THEN MONASTIC A WHITE SOLID LINE WHITE
- (11) PROPOSED THEN MONASTIC A WHITE SOLID LINE WHITE
- (12) PROPOSED THEN MONASTIC A WHITE SOLID LINE WHITE
- (13) PROPOSED THEN MONASTIC A WHITE SOLID LINE WHITE
- (14) PROPOSED THEN MONASTIC A WHITE SOLID LINE WHITE
- (15) PROPOSED THEN MONASTIC A WHITE SOLID LINE WHITE
- (16) PROPOSED THEN MONASTIC A WHITE SOLID LINE WHITE
- (17) PROPOSED THEN MONASTIC A WHITE SOLID LINE WHITE
- (18) PROPOSED THEN MONASTIC A WHITE SOLID LINE WHITE
- (19) PROPOSED THEN MONASTIC A WHITE SOLID LINE WHITE
- (20) PROPOSED THEN MONASTIC A WHITE SOLID LINE WHITE
- (21) PROPOSED THEN MONASTIC A WHITE SOLID LINE WHITE
- (22) PROPOSED THEN MONASTIC A WHITE SOLID LINE WHITE



PROPOSED BOUNDARY LINE

PROPOSED LOT LINE

EXISTING PROPERTY BOUNDARY LINE

EXISTING BOUNDARY LINE

FIRE ACCESS EASEMENT

SIDEWALK PAVEMENT

LANDSCAPE BUFFER

DORMING TO REMAIN

PROPOSED NEW

PROPOSED HEAVY DUTY ASPHALT PAVEMENT

PROPOSED 2" ASPHALT TAIL AND OVERLAY

PROPOSED CONCRETE PAVEMENT

[illegible][illegible][illegible]

BEARINGS ARE BASED ON THE NORTH LINE OF THE NORTHEAST QUARTER OF SECTION 34 AND MONUMENTED ON THE WEST BY A 2.5' ALUMINUM CAP STAMPED "P1529752" IN A RANGE BOX TO THE N 1/4 CORNER AND MONUMENTED ON THE EAST BY A 2.5' ALUMINUM CAP STAMPED "P1512485" FOR THE NE CORNER AND IS ASSIGNED TO BEAR S 69°50'1" E.

ELEVATIONS SHOWN ARE NAVD 88 AND ARE DERIVED VIA GPS OBSERVATIONS FROM THE IUGA SMARTNET RTK GPS NETWORK AND GEOID 128.

1. ALL UTILITIES SHOWN ARE BASED ON MAPS PROVIDED BY THE APPROPRIATE UTILITY COMPANY AND FIELD SURVEY. APPROXIMATE LOCATION ONLY. IT IS NOT TO BE CONSIDERED A REPRESENTATION TO FIELD VERIFY THE FIELD LOCATION OF ALL UTILITIES, PUBLIC OR PRIVATE, WHETHER SHOWN ON THE PLANS OR NOT. INQUIRY TO THE UTILITIES COMPANY FOR LOCATION OF UTILITIES IS THE ENGINEER PRIOR TO CONSTRUCTION.
2. THE CONTRACTOR'S RESPONSIBILITY TO FIELD VERIFY THE UTILITIES, PUBLIC AND PRIVATE, AND VERTICAL LOCATION OF SUCH UTILITIES, PRIOR TO CONSTRUCTION, IS THE ENGINEER PRIOR TO CONSTRUCTION.

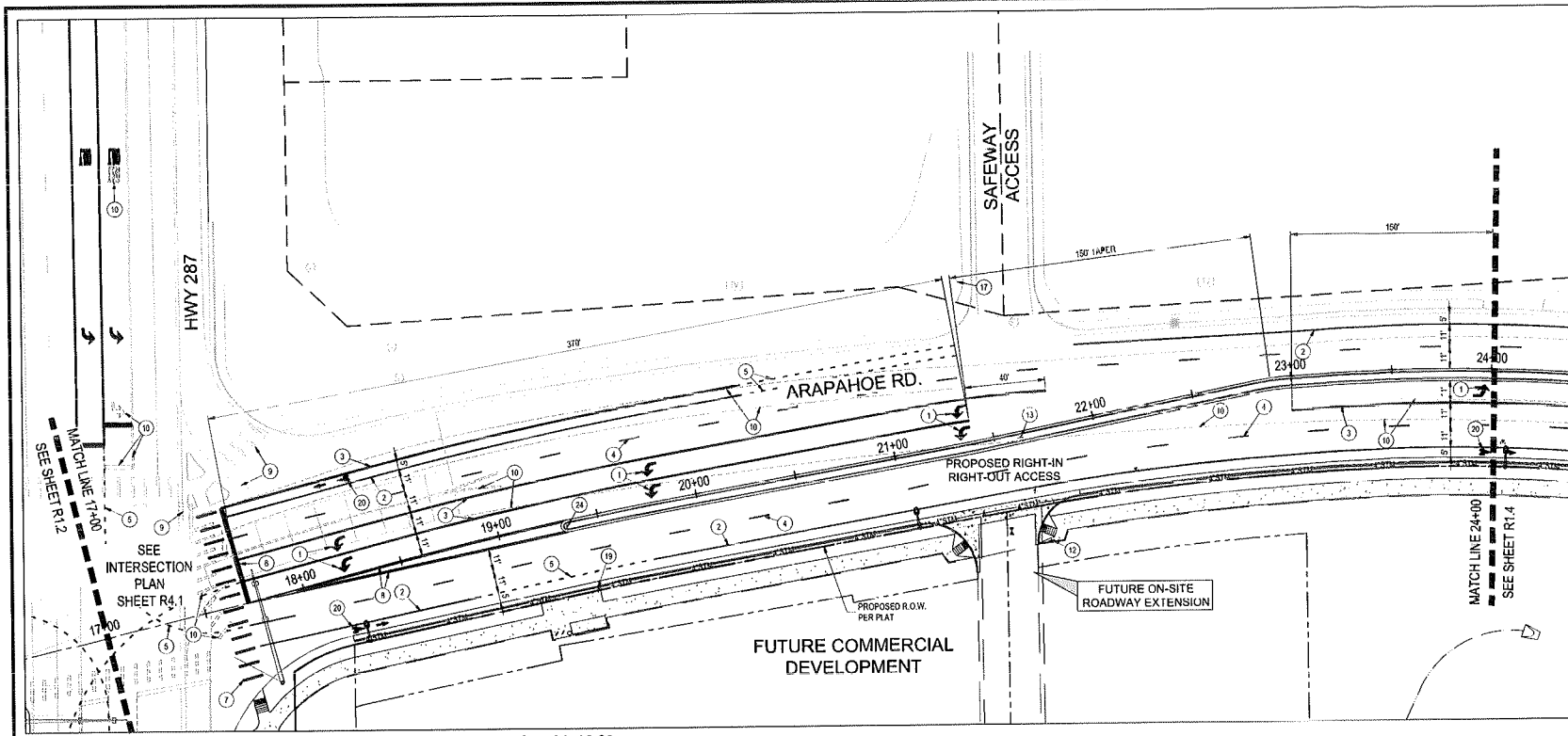


**City of Bellevue**  
**Call before you dig**  
 206.462.3333

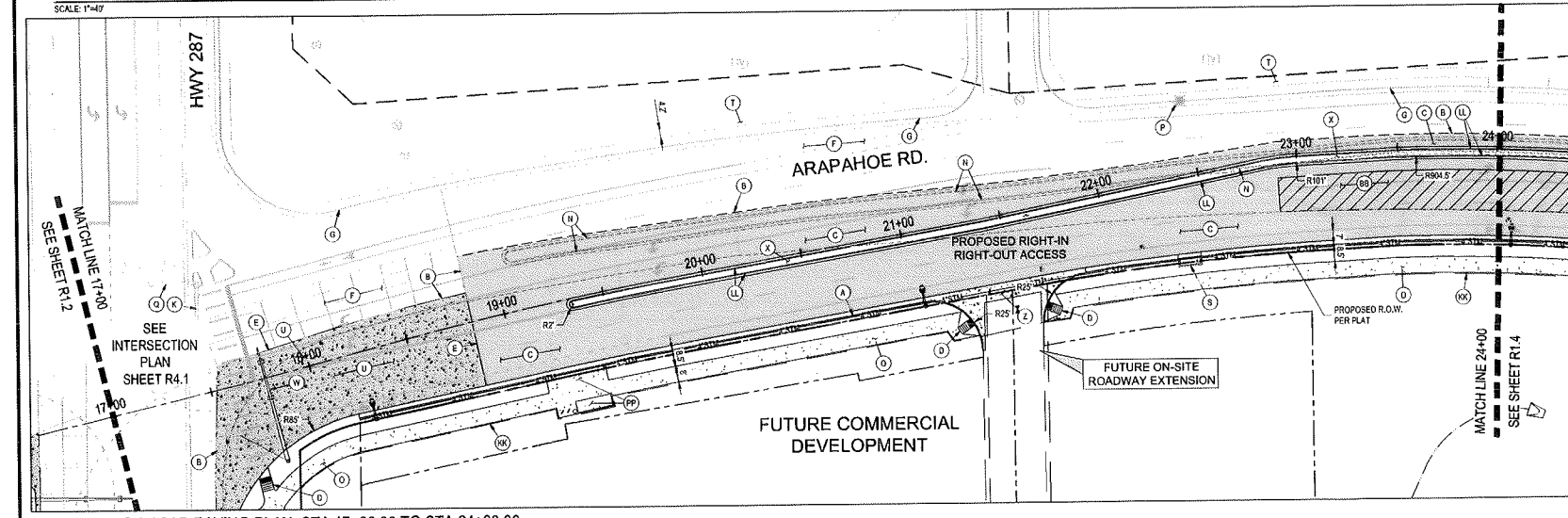
HIGHWAY 287 & ARAPAHOE RD.  
ERIE, COLORADO

#	Date	Issue / Description	Int.
1	11/6/91	4TH INFRASTRUC. SUB.	ACI
2	01/6/92	5TH INFRASTRUC. SUB.	ACI
3	02/20/92	6TH INFRASTRUC. SUB.	ACI
4	03/03/92	7TH INFRASTRUC. SUB.	ACI
5	03/17/92	FINAL STAMPED NF SET	BSM

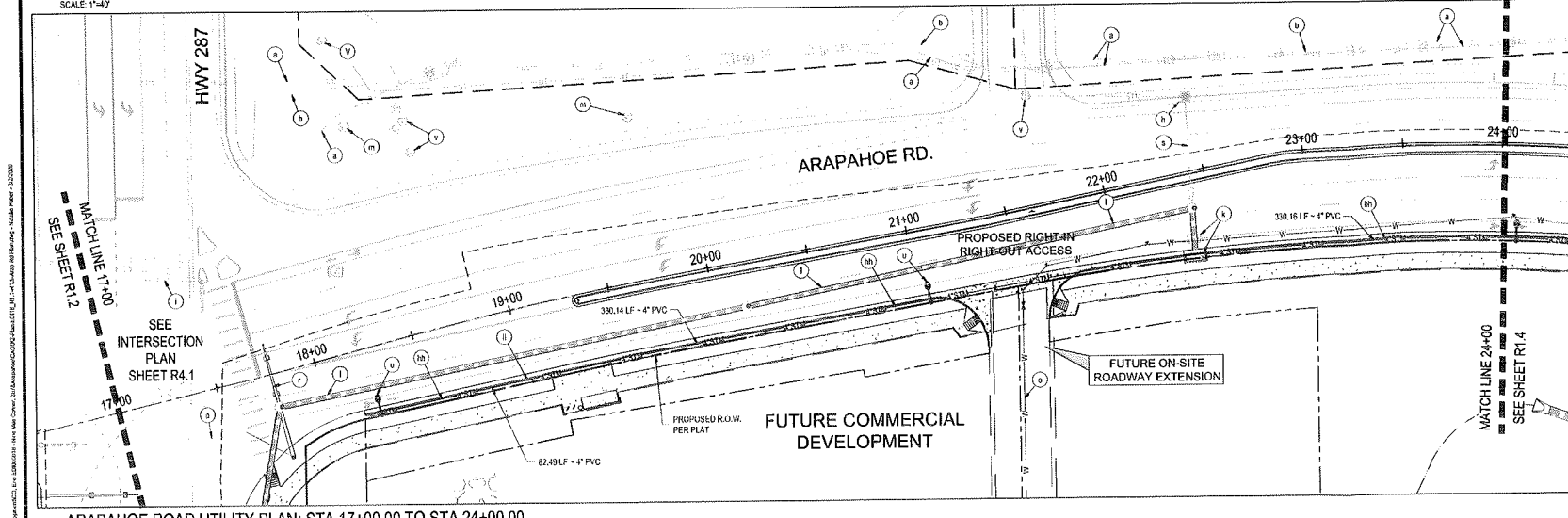
Project No:	ED1000018
Drawn By:	RDG
Checked By:	BSM
Date:	6/3/19



ARAPAHOE ROAD SIGNAGE & STRIPING PLAN: STA 17+00.00 TO STA 24+00.00  
SCALE: 1"=40'



ARAPAHOE ROAD PAVING PLAN: STA 17+00.00 TO STA 24+00.00  
SCALE: 1"=40'



ARAPAHOE ROAD UTILITY PLAN: STA 17+00.00 TO STA 24+00.00  
SCALE: 1"=40'

SIGNAGE & STRIPING SCHEDULE

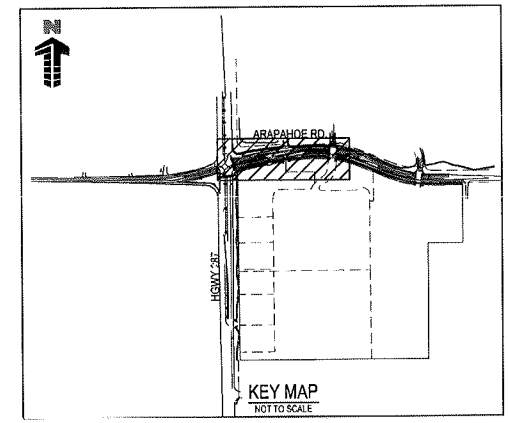
- 1) PROPOSED THERMOPLASTIC LEFT TURN SYMBOL, WHITE
- 2) PROPOSED THERMOPLASTIC 4" WIDE SOLID LINE, WHITE
- 3) PROPOSED THERMOPLASTIC 6" WIDE SOLID LINE, WHITE
- 4) PROPOSED THERMOPLASTIC 4" WIDE SKIP LANE LINE, WHITE
- 5) PROPOSED THERMOPLASTIC 4" WIDE DOTTED EXTENSION LINE, WHITE
- 6) PROPOSED THERMOPLASTIC 24" WIDE STOP BAR, WHITE
- 7) PROPOSED THERMOPLASTIC CROSSWALK STRIPING PER MUTCD, WHITE
- 8) PROPOSED 4" DOUBLE SOLID YELLOW LINES
- 9) EXISTING PAVEMENT MARKING TO REMAIN
- 10) EXISTING PAVEMENT MARKING TO BE REMOVED
- 11) PROPOSED 'ONLY' TEXT, WHITE
- 12) PROPOSED 'STOP' SIGN (PER MUTCD R1-1 U-1A)
- 13) PROPOSED 'ONE WAY' SIGN (PER MUTCD R5-1R DETAIL)
- 14) PROPOSED 4" WIDE SOLID LINE, YELLOW
- 15) PROPOSED 8" WIDE DOTTED EXTENSION LINE, WHITE
- 16) PROPOSED RIGHT TURN SYMBOL, WHITE
- 17) EXISTING 'STOP' SIGN TO REMAIN
- 18) PROPOSED 'RIGHT LANE MUST TURN RIGHT' SIGN (PER MUTCD R3-7R DETAIL)
- 19) PROPOSED DUG STOP SIGN, BUS STOP LOCATION TO BE CONFINED WITH RTM
- 20) PROPOSED BIKELANE SYMBOL, WHITE
- 21) PROPOSED ARROW SYMBOL, WHITE
- 22) EXISTING SIGN TO REMAIN
- 23) EXISTING SIGN TO BE RELOCATED
- 24) PROPOSED KEEP RIGHT MEDIAN SIGN (PER MUTCD R4-7 DETAIL)

PAVING PLAN SCHEDULE

- A) PROPOSED 6" CURB AND 2" GUTTER (REF SHEET R5.2)
- B) PROPOSED SAWCUT
- C) PROPOSED ASPHALT PAVEMENT (REF SHEET R5.2)
- D) PROPOSED ADA RAMP (REF SHEET R5.2)
- E) PROPOSED EDGE OF CONCRETE
- F) EXISTING ROADWAY PAVEMENT TO REMAIN
- G) EXISTING CURB AND GUTTER TO REMAIN
- H) EXISTING EDGE OF ASPHALT TO REMAIN
- I) EXISTING ADA RAMP TO REMAIN
- J) EXISTING TRAFFIC SIGNAL TO BE REMOVED AND RELOCATED
- K) EXISTING TRAFFIC SIGNAL EQUIPMENT TO BE REMOVED AND RELOCATED
- L) EXISTING TRAFFIC SIGNAL EQUIPMENT TO BE REMOVED AND RELOCATED
- M) EXISTING MONUMENT TO REMAIN, COORD. W/ BOULDER COUNTY ON PROTECTION PRIOR TO CONSTRUCTION
- N) EXISTING CURB AND GUTTER TO BE REMOVED
- O) PROPOSED SIDEWALK (REF SHEET R5.2)
- P) EXISTING INLET TO REMAIN
- Q) EXISTING TRAFFIC SIGNAL POLE AND MAST ARM TO REMAIN
- R) EXISTING STREET LIGHT TO REMAIN
- S) EXISTING INLET TO BE REMOVED AND REPLACED WITH TYPE R INLET
- T) EXISTING SIDEWALK TO REMAIN
- U) PROPOSED CONCRETE PAVEMENT TO MATCH EXISTING JOINTING (REF SHEET R3.1 & R5.2)
- V) EXISTING EDGE OF CONCRETE TO REMAIN
- W) PROPOSED TRAFFIC SIGNAL POLE AND MAST ARM (REF SIGNAL PLANS)
- X) PROPOSED STAMPED 'BRICKFORM' CONCRETE MEDIAN WITH TILE RED COLOR HARDENER, TWO PARTS 'STONE GRAY' ANTIQUING AGENT, ONE PART 'DEEP CHARCOAL'
- Y) PROPOSED LOCATION OF RELOCATED TRAFFIC SIGNAL EQUIPMENT (REF SIGNAL PLANS)
- Z) PROPOSED 8-FT WIDE CONCRETE CROSSSPAN (REF SHEET R5.2)
- AA) PROPOSED TYPE III RIP RAP PAD (R5.6)
- BB) PROPOSED 2" ASPHALT MILL AND OVERLAY
- CC) PROPOSED TYPE 3 W-BEAM GUARDRAIL (REF SHEET R5.2)
- DD) PROPOSED RAILING (BY OTHERS)
- EE) EXISTING FENCE TO REMAIN
- FF) EXISTING FENCE TO BE REMOVED
- GG) EXISTING TELEPHONE VAULT TO REMAIN
- HH) EXISTING TELEPHONE VAULT TO BE RELOCATED
- II) EXISTING STORM SEWER
- JJ) EXISTING UNDERGROUND GAS LINE
- KK) EXISTING UNDERGROUND ELECTRICAL
- LL) EXISTING FIBER OPTIC LINE
- MM) EXISTING FIBER OPTIC LINE
- NN) EXISTING FIBER OPTIC LINE
- OO) EXISTING FIBER OPTIC LINE
- PP) EXISTING FIBER OPTIC LINE
- QQ) EXISTING FIBER OPTIC LINE
- RR) EXISTING FIBER OPTIC LINE
- SS) EXISTING FIBER OPTIC LINE
- TT) EXISTING FIBER OPTIC LINE
- UU) EXISTING FIBER OPTIC LINE
- VV) EXISTING FIBER OPTIC LINE
- WW) EXISTING FIBER OPTIC LINE
- XX) EXISTING FIBER OPTIC LINE
- YY) EXISTING FIBER OPTIC LINE
- ZZ) EXISTING FIBER OPTIC LINE

UTILITY PLAN SCHEDULE

- A) EXISTING UNDERGROUND ELECTRIC TO REMAIN
- B) EXISTING GAS LINE TO REMAIN
- C) EXISTING IRRIGATION LINE TO REMAIN
- D) EXISTING WATER MAIN TO REMAIN
- E) EXISTING TRAFFIC SIGNAL TO BE REMOVED AND RELOCATED
- F) EXISTING TRAFFIC SIGNAL EQUIPMENT TO BE REMOVED AND RELOCATED
- G) PROPOSED STORM SEWER INLET (SEE STORM SEWER PLAN & PROFILES)
- H) EXISTING STORM SEWER INLET TO REMAIN
- I) EXISTING TRAFFIC SIGNAL POLE AND MAST ARM TO REMAIN
- J) EXISTING STREET LIGHT TO REMAIN
- K) EXISTING INLET TO BE REMOVED AND REPLACED WITH TYPE R INLET
- L) PROPOSED STORM SEWER (SEE STORM SEWER PLAN & PROFILES)
- M) EXISTING SANITARY MANHOLE TO REMAIN
- N) PROPOSED SANITARY SEWER (SEE SANITARY SEWER PLAN & PROFILES)
- O) PROPOSED WATER LINE (SEE WATER PLAN & PROFILES)
- P) EXISTING STORM SEWER TO REMAIN
- Q) EXISTING UNDERGROUND TELEPHONE LINE TO REMAIN
- R) PROPOSED TRAFFIC SIGNAL POLE AND MAST ARM (SEE SIGNAL PLANS)
- S) EXISTING 24-INCH STORM SEWER TO REMAIN
- T) PROPOSED LOCATION OF RELOCATED TRAFFIC SIGNAL EQUIPMENT (SEE SIGNAL PLANS)
- U) PROPOSED STREET LIGHT, GREEN COBRALED STREET LIGHT (SEE SHEET R5.3)
- V) EXISTING STORM SEWER MANHOLE TO REMAIN
- W) PROPOSED 1/2" BOX CULVERT 16.5' - LF EXTENSION
- X) EXISTING BOX CULVERT TO REMAIN
- Y) EXISTING POWER POLE TO REMAIN
- Z) EXISTING POWER POLE TO BE RELOCATED
- aa) EXISTING WATER METER TO REMAIN
- bb) EXISTING POWER POLE AND METER TO BE RELOCATED
- cc) EXISTING OVERHEAD ELECTRIC LINE TO BE REMOVED AND RELOCATED
- dd) 40' UTILITY EASEMENT BY SEPARATE DOCUMENT
- ee) EXISTING WATER LINE TO BE REMOVED
- ff) PROPOSED STORM SEWER INLET (BY OTHERS)
- gg) PROPOSED STORM SEWER MANHOLE (BY OTHERS)
- hh) PROPOSED CURB DRAIN (SEE ROADWAY DETAIL)
- ii) PROPOSED TWO-WAY CLEANOUT



ROADWAY LEGEND

- PROPERTY BOUNDARY LINE
- PROPOSED LOT LINE
- ADJACENT PROPERTY BOUNDARY LINE
- EASEMENT BOUNDARY LINE
- FIRE ACCESS EASEMENT
- SIDEWALK EASEMENT
- LANDSCAPE BUFFER
- EXISTING TO REMAIN
- PROPOSED NEW
- PROPOSED HEAVY DUTY ASPHALT PAVEMENT
- PROPOSED 2" ASPHALT MILL AND OVERLAY
- PROPOSED CONCRETE PAVEMENT

UTILITY LEGEND

- EXISTING MINOR CONTOUR
- EXISTING MAJOR CONTOUR
- PROPOSED MINOR CONTOUR
- PROPOSED MAJOR CONTOUR
- PROPERTY BOUNDARY LINE
- PROPOSED LOT LINE
- ADJACENT PROPERTY BOUNDARY LINE
- EASEMENT BOUNDARY LINE
- EXISTING WATER LINE
- EXISTING SANITARY SEWER
- EXISTING TELEPHONE CABINET TO BE RELOCATED
- EXISTING UNDERGROUND GAS LINE
- EXISTING UNDERGROUND ELECTRICAL
- EXISTING UNDERGROUND TELEPHONE
- EXISTING FIBER OPTIC LINE
- STORM SEWER BY OTHERS (LESS THAN 12')
- STORM SEWER BY OTHERS (GREATER THAN 12')
- SANITARY SEWER BY OTHERS
- WATER LINE BY OTHERS
- PROPOSED WATER LINE
- PROPOSED SANITARY SEWER
- PROPOSED STORM SEWER (GREATER THAN 12')
- PROPOSED UNDERGROUND GAS LINE
- PROPOSED UNDERGROUND ELECTRICAL
- PROPOSED UNDERGROUND TELEPHONE
- PROPOSED GUARDRAIL
- EXISTING WATER VALVE
- EXISTING MANHOLE
- PROPOSED WATER METER
- PROPOSED WATER VALVE
- PROPOSED FIRE HYDRANT
- PROPOSED MANHOLE
- UTILITY PEDESTAL
- PROPOSED ELECTRICAL TRANSFORMER
- PROPOSED INLET

BASIS OF BEARING

BEARINGS ARE BASED ON THE NORTH LINE OF THE NORTHEAST QUARTER OF SECTION 34 AND MONUMENTED ON THE WEST BY A 2.5" ALUMINUM CAP STAMPED 'PLS287S2' IN A RANGE BOX FOR THE N1/4 CORNER AND MONUMENTED ON THE EAST BY A 2.5" ALUMINUM CAP STAMPED 'PLS180S2' FOR THE E1/4 CORNER AND IS ASSUMED TO BEAR S 89° 50' 00" E.

BENCHMARK

ELEVATIONS SHOWN ARE NAVD 88 AND ARE DERIVED VIA GPS OBSERVATIONS FROM THE UICCA SMARTNET RTK GPS NETWORK AND GEOID 128.

CAUTION - NOTICE TO CONTRACTOR

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- WHERE A PROPOSED UTILITY CROSSES AN EXISTING UTILITY, IT IS THE CONTRACTOR'S RESPONSIBILITY TO FIELD VERIFY THE HORIZONTAL AND VERTICAL LOCATION OF SUCH EXISTING UTILITY, EITHER THROUGH POT-HOILING OR ALTERNATIVE METHOD. REPORT INFORMATION TO THE ENGINEER PRIOR TO CONSTRUCTION.

**Galloway**  
8162 S. Willow Drive, Suite 320  
Greenwood Village, CO 80111  
303.770.8884  
GallowayUS.com

**Evergreen**  
Development | Services | Investments  
GOVERNMENT LICENSED PROFESSIONAL ENGINEER  
40216  
3/17/2020

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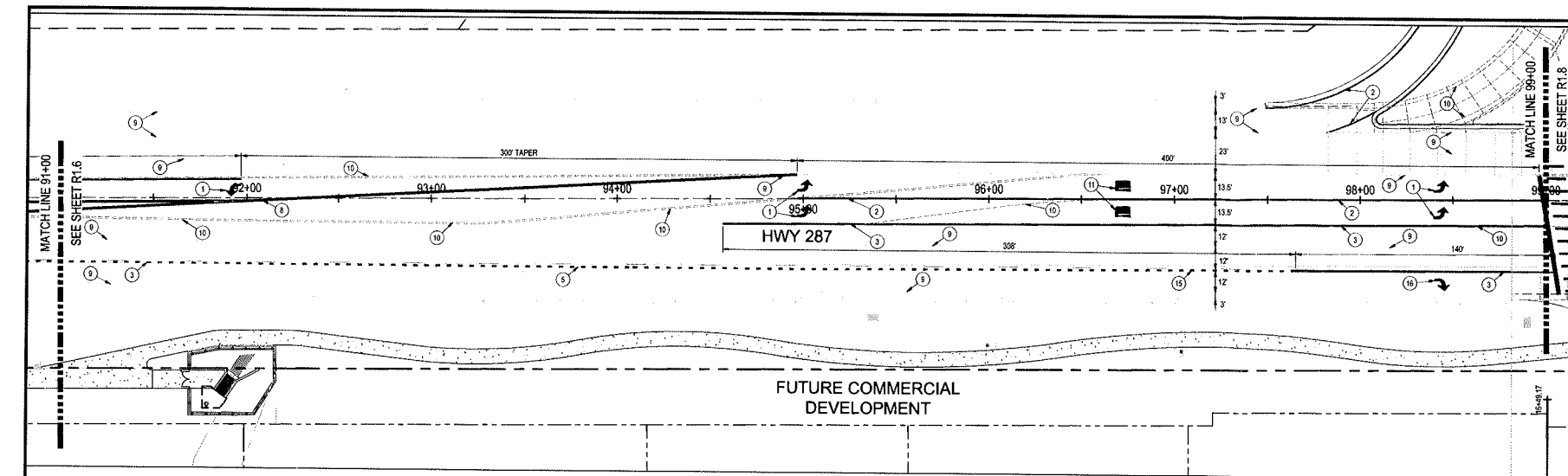
**INFRASTRUCTURE CONSTRUCTION DOCUMENTS**  
NINE MILE CORNER  
EVERGREEN DEVCO INC  
HIGHWAY 287 & ARAPAHOE RD.  
ERIE, COLORADO

#	Date	Issue / Description	Init.
1	11/15/19	4TH INFRASTRUC. SUB.	ACJ
2	01/24/20	5TH INFRASTRUC. SUB.	ACJ
3	02/20/20	6TH INFRASTRUC. SUB.	ACJ
4	03/03/20	7TH INFRASTRUC. SUB.	ACJ
5	03/17/20	FINAL STAMPED INF. SET	BSM

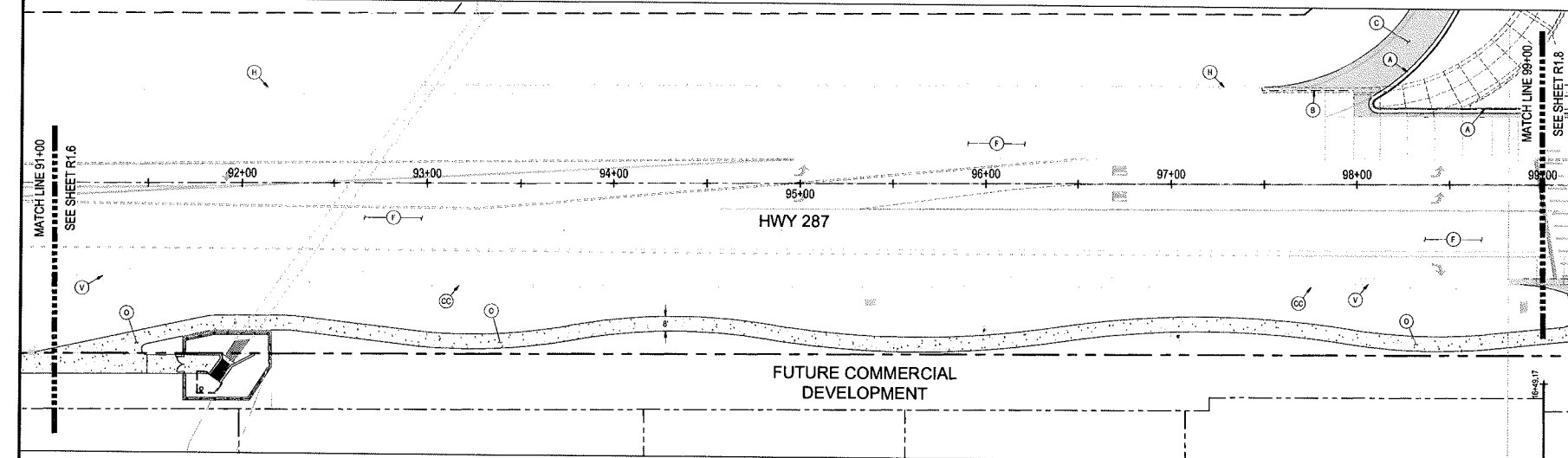
Project No:	ED0000018
Drawn By:	RDG
Checked By:	BSM
Date:	9/3/19

ARAPAHOE ROAD SITE PLAN

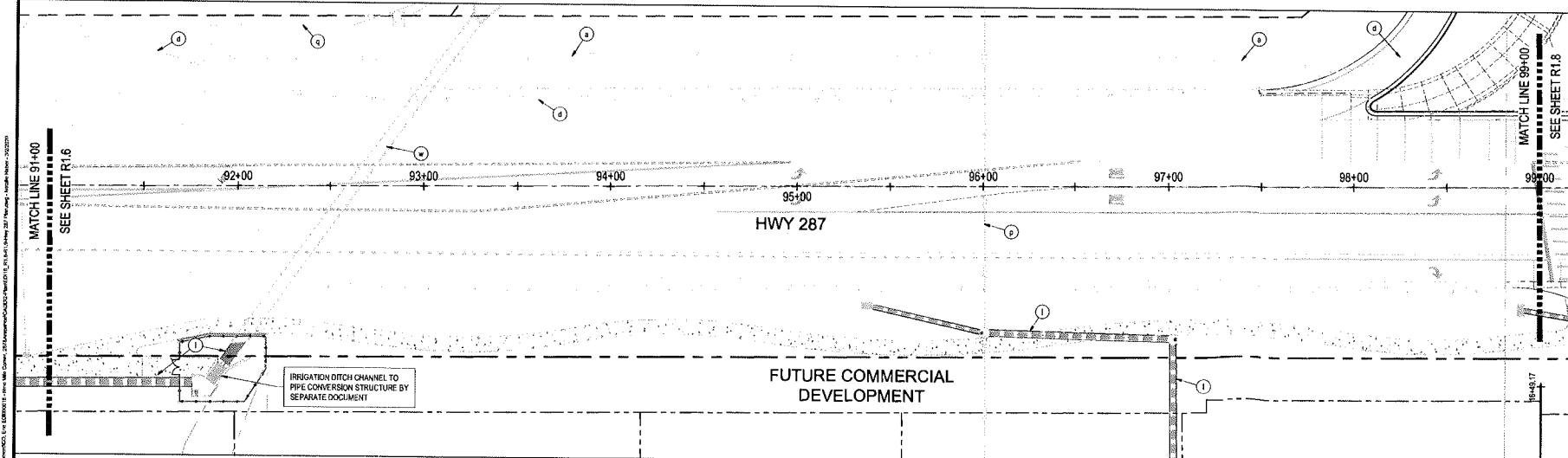
**R1.3**



HIGHWAY 287 SIGNAGE & STRIPING PLAN: STA 91+00.00 TO STA 99+00.00  
SCALE: 1"=40'



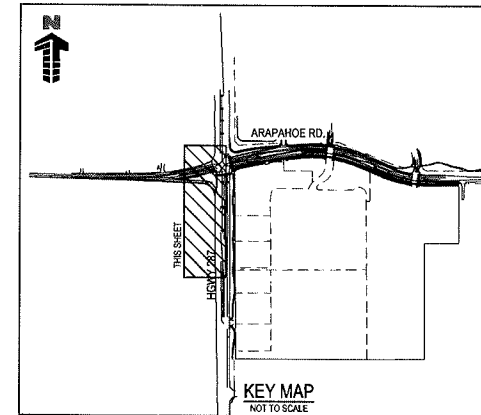
HIGHWAY 287 PAVING PLAN: STA 91+00.00 TO STA 99+00.00  
SCALE: 1"=40'



HIGHWAY 287 UTILITY PLAN: STA 91+00.00 TO STA 99+00.00  
SCALE: 1"=40'

## SIGNAGE & STRIPING SCHEDULE

- PROPOSED LEFT TURN SYMBOL, WHITE
- PROPOSED 4" WIDE SOLID LINE, WHITE
- PROPOSED 6" WIDE SOLID LINE, WHITE
- PROPOSED 4" WIDE SKIP LANE LINE, WHITE
- PROPOSED 4" WIDE DOTTED EXTENSION LINE, WHITE
- PROPOSED 24" WIDE STOP BAR, WHITE
- PROPOSED CROSSWALK STRIPING PER M.U.T.C.D., WHITE
- PROPOSED 4" DOUBLE SOLID YELLOW LINES
- EXISTING PAVEMENT MARKING TO REMAIN
- EXISTING PAVEMENT MARKING TO BE REMOVED
- PROPOSED "ONLY" TEXT, WHITE
- PROPOSED "STOP" SIGN
- PROPOSED "ONE WAY" SIGN
- PROPOSED 4" WIDE SOLID LINE, YELLOW
- PROPOSED 6" WIDE DOTTED EXTENSION LINE, WHITE
- PROPOSED RIGHT TURN SYMBOL, WHITE
- EXISTING "STOP" SIGN TO REMAIN
- PROPOSED "RIGHT LANE MUST TURN RIGHT" SIGN
- PROPOSED BUS STOP SIGN, BUS STOP LOCATION TO BE CONFIRMED WITH RTD



## PAVING PLAN SCHEDULE

- PROPOSED 6" CURB AND 2" GUTTER
- PROPOSED SAWCUT
- PROPOSED ASPHALT PAVEMENT
- PROPOSED ADA RAMP
- PROPOSED EDGE OF CONCRETE
- EXISTING ROADWAY PAVEMENT TO REMAIN
- EXISTING CURB AND GUTTER TO REMAIN
- EXISTING EDGE OF ASPHALT TO REMAIN
- EXISTING ADA RAMP TO REMAIN
- EXISTING TRAFFIC SIGNAL TO BE REMOVED AND RELOCATED
- EXISTING TRAFFIC SIGNAL EQUIPMENT TO BE REMOVED AND RELOCATED
- EXISTING MONUMENT TO REMAIN
- EXISTING CURB AND GUTTER TO BE REMOVED
- PROPOSED SIDEWALK
- EXISTING INLET TO REMAIN
- EXISTING TRAFFIC SIGNAL POLE AND MAST ARM TO REMAIN
- EXISTING STREET LIGHT TO REMAIN
- EXISTING INLET TO BE REMOVED AND REPLACED WITH TYPE R INLET
- EXISTING SIDEWALK TO REMAIN
- PROPOSED CONCRETE PAVEMENT
- EXISTING EDGE OF CONCRETE TO REMAIN
- PROPOSED TRAFFIC SIGNAL POLE AND MAST ARM
- PROPOSED STAMPED CONCRETE MEDIAN TO MATCH EXISTING
- PROPOSED LOCATION OF RELOCATED TRAFFIC SIGNAL EQUIPMENT
- PROPOSED 8-FT WIDE CONCRETE CROSSWALK
- PROPOSED RIP RAP
- PROPOSED ASPHALT MILL AND OVERLAY
- EXISTING GUARDRAIL TO REMAIN

## UTILITY PLAN SCHEDULE

- EXISTING UNDERGROUND ELECTRIC TO REMAIN
- EXISTING GAS LINE TO REMAIN
- EXISTING IRRIGATION LINE TO REMAIN
- EXISTING WATER MAIN TO REMAIN
- EXISTING TRAFFIC SIGNAL TO BE REMOVED AND RELOCATED
- EXISTING TRAFFIC SIGNAL EQUIPMENT TO BE REMOVED AND RELOCATED
- PROPOSED STORM SEWER INLET (SEE STORM SEWER PLAN & PROFILES)
- EXISTING STORM SEWER INLET TO REMAIN
- EXISTING TRAFFIC SIGNAL POLE AND MAST ARM TO REMAIN
- EXISTING STREET LIGHT TO REMAIN
- EXISTING INLET TO BE REMOVED AND REPLACED WITH TYPE R INLET
- PROPOSED STORM SEWER (SEE STORM SEWER PLAN & PROFILES)
- EXISTING SANITARY MANHOLE TO REMAIN
- PROPOSED SANITARY SEWER (SEE SANITARY SEWER PLAN & PROFILES)
- PROPOSED WATER LINE (SEE WATER PLAN)
- EXISTING STORM SEWER TO REMAIN
- EXISTING UNDERGROUND TELEPHONE LINE TO REMAIN
- PROPOSED TRAFFIC SIGNAL POLE AND MAST ARM
- EXISTING 24-INCH STORM SEWER TO REMAIN
- PROPOSED LOCATION OF RELOCATED TRAFFIC SIGNAL EQUIPMENT
- PROPOSED STREET LIGHT, 250-WATT HIGH PRESSURE SODIUM, 35-FT IN HEIGHT
- EXISTING STORM SEWER MANHOLE TO REMAIN
- EXISTING BOX CULVERT UNDER HIGHWAY 287
- EXISTING BOX CULVERT TO REMAIN
- EXISTING POWER POLE TO REMAIN
- EXISTING POWER POLE TO BE RELOCATED
- EXISTING POWER POLE TO BE RELOCATED
- EXISTING INLET TO BE REMOVED AND REPLACED WITH TYPE R INLET
- 48" UTILITY EASEMENT
- EXISTING WATER LINE TO BE REMOVED
- PROPOSED STORM SEWER INLET (BY OTHERS)
- PROPOSED STORM SEWER MANHOLE (BY OTHERS)

## ROADWAY LEGEND

---	PROPERTY BOUNDARY LINE
---	PROPOSED LOT LINE
---	ADJACENT PROPERTY BOUNDARY LINE
---	EASEMENT BOUNDARY LINE
---	FIRE ACCESS EASEMENT
---	SIDEWALK EASEMENT
---	LANDSCAPE BUFFER
---	EXISTING TO REMAIN
---	PROPOSED FUTURE
---	PROPOSED NEW
---	PROPOSED HEAVY DUTY ASPHALT PAVEMENT
---	PROPOSED 2" ASPHALT MILL AND OVERLAY
---	PROPOSED CONCRETE PAVEMENT

## UTILITY LEGEND

---	EXISTING MINOR CONTOUR
---	EXISTING MAJOR CONTOUR
---	PROPOSED MINOR CONTOUR
---	PROPOSED MAJOR CONTOUR
---	PROPERTY BOUNDARY LINE
---	PROPOSED LOT LINE
---	ADJACENT PROPERTY BOUNDARY LINE
---	EASEMENT BOUNDARY LINE
---	EXISTING WATER LINE
---	EXISTING SANITARY SEWER
---	EXISTING STORM SEWER
---	EXISTING UNDERGROUND GAS LINE
---	EXISTING UNDERGROUND ELECTRICAL
---	EXISTING UNDERGROUND TELEPHONE
---	EXISTING FIBER OPTIC LINE
---	STORM SEWER BY OTHERS (LESS THAN 12')
---	STORM SEWER BY OTHERS (GREATER THAN 12')
---	SANITARY SEWER BY OTHERS
---	WATER LINE BY OTHERS
---	PROPOSED WATER LINE
---	PROPOSED SANITARY SEWER
---	PROPOSED STORM SEWER (GREATER THAN 12')
---	PROPOSED UNDERGROUND GAS LINE
---	PROPOSED UNDERGROUND ELECTRICAL
---	PROPOSED UNDERGROUND TELEPHONE
---	PROPOSED GUARDRAIL
---	EXISTING WATER VALVE
---	EXISTING MANHOLE
---	PROPOSED WATER METER
---	PROPOSED WATER VALVE
---	PROPOSED FIRE HYDRANT
---	PROPOSED MANHOLE
---	UTILITY PEDESTAL
---	PROPOSED ELECTRICAL TRANSFORMER
---	PROPOSED INLET

## BASIS OF BEARING

BEARINGS ARE BASED ON THE NORTH LINE OF THE NORTHEAST QUARTER OF SECTION 34 AND MONUMENTED ON THE WEST BY A 2.5" ALUMINUM CAP STAMPED "PLS29292" IN A RANGE BOX FOR THE N 1/4 CORNER AND MONUMENTED ON THE EAST BY A 2.5" ALUMINUM CAP STAMPED "PLS12000" FOR THE NE CORNER AND IS ASSUMED TO BEAR S 89°58'11" E.

## BENCHMARK

ELEVATIONS SHOWN ARE NAVD 83 AND ARE DERIVED VIA GPS OBSERVATIONS FROM THE LIECA SMARTNET RTK GPS NETWORK AND GEOID 12B.

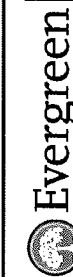
## CAUTION - NOTICE TO CONTRACTOR

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**Galloway**

6162 S. Willow Drive, Suite 320  
Greenwood Village, CO 80111  
303.770.8884  
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INFRASTRUCTURE CONSTRUCTION DOCUMENTS

NINE MILE CORNER  
EVERGREEN DEVCO INC

HIGHWAY 287 & ARAPAHOE RD.  
ERIE, COLORADO

#	Date	Issue / Description	Init.
1	11/15/19	4TH INFRASTRUC. SUB.	ACJ
2	01/24/20	5TH INFRASTRUC. SUB.	ACJ
3	02/28/20	6TH INFRASTRUC. SUB.	ACJ
4	03/03/20	7TH INFRASTRUC. SUB.	ACJ
5	03/17/20	FINAL STAMPED INF. SET	BSM

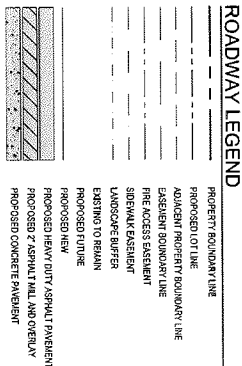
Project No:	ED1000018
Drawn By:	RDG
Checked By:	BSM
Date:	9/3/19

HIGHWAY 287 SITE PLAN

**R1.7**

INFRASTRUCTURE CONSTRUCTION  
DOCUMENTS  
NINE MILE CORNER  
EVERGREEN DEVCO INC

HIGHWAY 287 & ARAPAHOE RD.  
ERIE, COLORADO



54

5455

PROPOSED MAJOR CONTOUR

PROPOSED LOT LINE

ADJACENT PROPERTY BOUNDARY LINE

EASEMENT BOUNDARY LINE

10th St

11th St

54th St

55th St

EXISTING UNDERGROUND GAS LINE	EXISTING UNDERGROUND ELECTRICAL	EXISTING UNDERGROUND TELEPHONE	EXISTING FIBER OPTIC LINE	STORM SEWER BY OTHERS (LESS THAN 12")	STORM SEWER BY OTHERS (GREATER THAN 12")
0.1	0.2	0.2	0.1	0.1	0.1

55	PROPOSED SPINNY SPINER
1	PROPOSED UNDERGROUND OIL LINE
2	PROPOSED UNDERGROUND GAS LINE
3	PROPOSED UNDERGROUND ELECTRICAL
4	PROPOSED UNDERGROUND TELEPHONE
5	PROPOSED CABLESTAY
6	EXISTING WATER VALVE
7	PROPOSED WATER VALVE
8	PROPOSED FIRE HYDRANT
9	PROPOSED MANHOLE
10	UTILITY RESTRUT
11	PROPOSED CLOTTINAL TRANSFORMER
12	PROPOSED INLET

**BASIS OF BEARING**

BEARINGS ARE BASED ON THE NORTH LINE OF THE NORTHEAST QUARTER OF SECTION 34 AND MONUMENTED ON THE WEST BY A 2.5' ALUMINUM CAP STAMPED "T-59181" IN A RANGE BOX FOR THE N 1/4 CORNER AND MONUMENTED ON THE EAST BY A 2.5' ALUMINUM CAP STAMPED "T-512465" FOR THE NE CORNER AND IS ASSURED TO BEAR S 88°50'1" E.

## BENCHMARK

ELEVATIONS SHOWN ARE NAVD 89 AND ARE DERIVED VIA GPS OBSERVATIONS FROM THE ILECA SMARTNET RTK GPS NETWORK AND GEOID 12B.

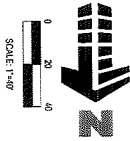
**CAUTION - NOTICE TO CONTRACTOR**

1. ALL UTILITY LOCATIONS SHOWN ARE BASED ON RECORDS PROVIDED BY THE APPROPRIATE UTILITY COMPANIES AND FIELD SURVEY EVIDENCE IN THE FIELD. SURETIES WILL NOT BE CONSIDERED AS RESPONSIBLE TO RECONSTRUCT THE FIELD LOCATION OF ALL UTILITIES, PUBLIC OR PRIVATE, WHETHER SHOWN ON THE PLANS OR NOT, PRIOR TO CONSTRUCTION. REPORT ANY DISCREPANCIES TO THE ENGINEERING FIRM TO CORRECT FUNCTION.
2. WHERE A PREEXISTING UTILITY CROSSES AN EXISTING UTILITY IT IS THE CONTRACTORS RESPONSIBILITY TO FIND, EXPOSE THE HORIZONTAL AND VERTICAL LOCATION OF SUCH CROSSINGS AND PROVIDE PROTECTIVE MEASURES TO MAINTAIN THE EXISTING UTILITY THROUGHOUT THE CONSTRUCTION PERIOD TO THE ENDED PERIOD FOR COMPLETION.

**811**

**Know what's below  
Call before you dig**

**811**  
Know what's below.  
Call before you dig.

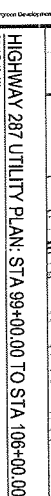
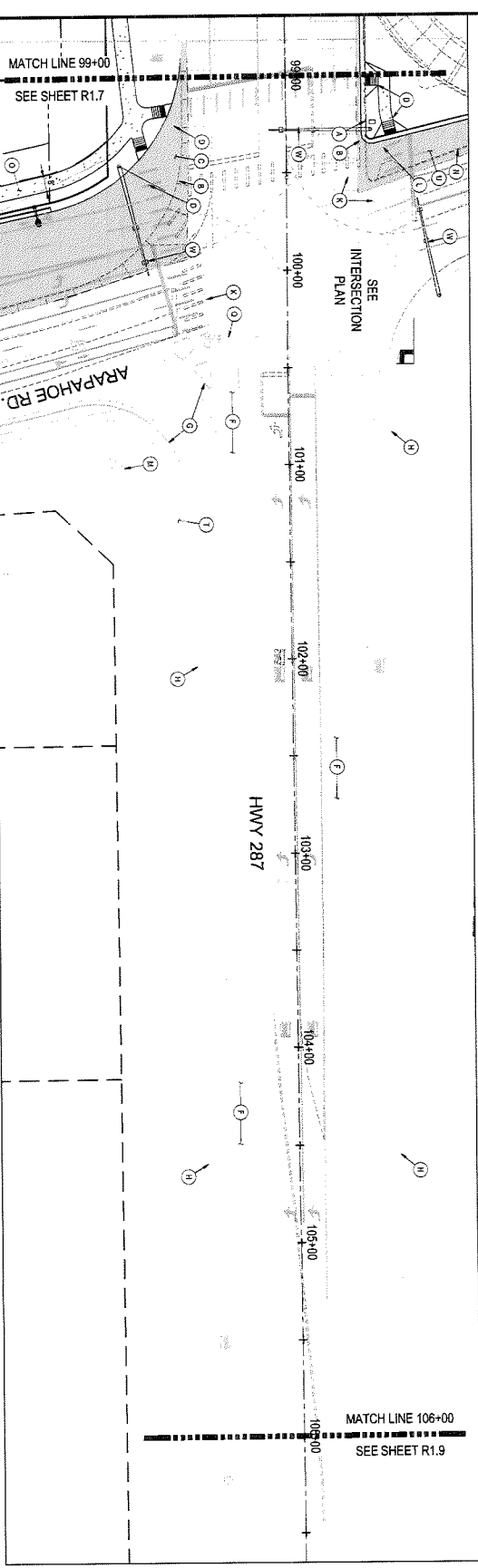
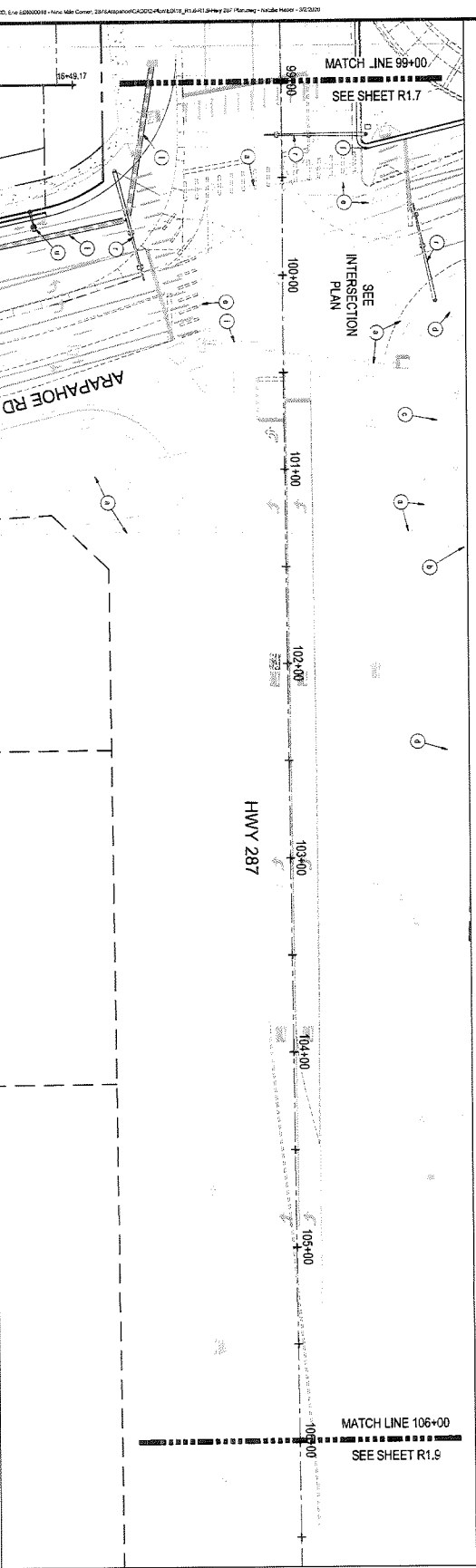


## PAVING PLAN SCHEDULE

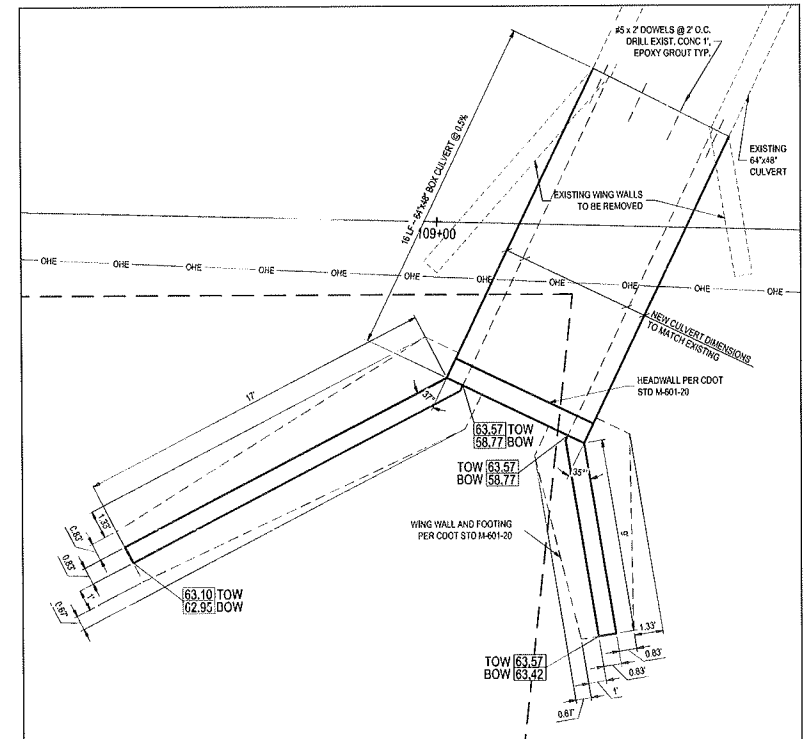
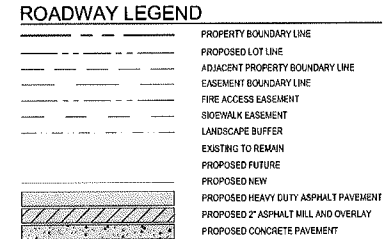
- |      |  |
|------|--|
| (A)  | PROPOSED CURB AND GUTTER   |
| (B)  | PROPOSED SARCOT  |
| (C)  | PROPOSED SIGNAL PAVEMENT   |
| (D)  | PROPOSED ADA RAMP  |
| (E)  | PROPOSED EDGE OF CONCRETE  |
| (F)  | EXISTING ROADWAY PARALLEL TO REMAIN                              |
| (G)  | EXISTING CURB AND GUTTER TO REMAIN                               |
| (H)  | EXISTING EDGE OF ASPHALT TO REMAIN                               |
| (I)  | EXISTING ADA RAMP TO REMAIN                                      |
| (J)  | EXISTING TRAFFIC SIGNAL IS TO BE REMOVED AND RELOCATED           |
| (K)  | EXISTING TRAFFIC SIGNAL EQUIPMENT IS TO BE REMOVED AND RELOCATED |
| (L)  | EXISTING MONUMENT TO REMAIN                                      |
| (M)  | EXISTING CURB AND GUTTER TO BE REMOVED                           |
| (N)  | PROPOSED SIDEWALK  |
| (O)  | EXISTING INLET TO REMAIN   |
| (P)  | EXISTING TRAFFIC SIGNAL POLE AND MAST ARM TO REMAIN              |
| (Q)  | EXISTING STREET LIGHT TO REMAIN                                  |
| (R)  | EXISTING LEAKY TO BE REMOVED AND REPLACED WITH TYPE R INLET      |
| (S)  | EXISTING DRAINAGE TO REMAIN                                      |
| (T)  | EXISTING DRIVEWAY TO REMAIN                                      |
| (U)  | PROPOSED CONCRETE PARALLEL                                       |
| (V)  | PROPOSED EDGE OF CONCRETE TO REMAIN                              |
| (W)  | EXISTING TRAFFIC SIGNAL POLE AND MAST ARM                        |
| (X)  | PROPOSED STAGGERED CONCRETE MEDIAN IN MATCH EXISTING             |
| (Y)  | PROPOSED LOCATION OF RELOCATED TRAFFIC SIGNAL EQUIPMENT          |
| (Z)  | PROPOSED 8 FT WIDE CONCRETE CROSSSPAN                            |
| (AA) | PROPOSED BP PIPE   |
| (AB) | PROPOSED ASPHALT DRILL AND OVERLAY                               |
| (AC) | EXISTING SUBGRADE TO REMAIN                                      |

### UTILITY PLAN SCHEDULE

3. EXISTING DRAINAGE ELECTRIC TO REMAIN
4. EXISTING D&E LINE TO REMAIN
5. EXISTING PROPOSED LINE TO REMAIN
6. EXISTING WATER MAIN TO REMAIN
7. EXISTING TRAFFIC SIGNAL TO BE REMOVED AND RELOCATED
8. EXISTING TRAFFIC SIGNAL EQUIPMENT TO BE REMOVED AND RELOCATED
9. PROPOSED STORM SEWER INLET (SEE STORM SEWER PLAN A) PROPOSED
10. EXISTING STORM SEWER MAIN TO REMAIN
11. EXISTING TRAFFIC SIGNAL POLE AND MAST MAIN TO REMAIN
12. EXISTING STREET LIGHT TO REMAIN
13. EXISTING L&E TO BE REMOVED AND SERVICED WITH TYPE R INLET
14. PROPOSED STORM SEWER (SEE STORM SEWER PLAN A) PROPOSED
15. EXISTING SANITARY MANHOLE TO REMAIN
16. PROPOSED SANITARY SEWER (SEE SANITARY SEWER PLAN A) PROPOSED
17. PROPOSED WATER LINE (SEE WATER PLAN A)
18. EXISTING STORM SEWER TO REMAIN
19. EXISTING UNDERGROUND TELEPHONE LINE TO REMAIN
20. PROPOSED TRAFFIC SIGNAL IN-PAVEMENT AND MAST AND EXISTING SIGNAL STORM SEWER TO REMAIN
21. EXISTING SIGNAL STORM SEWER TO REMAIN
22. PROPOSED STREET LIGHT, 30' MOUNT HIGH PRESSURE SIGNAL, 3.5" TYPE R INLET
23. EXISTING STORM SEWER MANHOLE TO REMAIN
24. EXISTING GROUND CULVERT UNDER HIGHWAY 387
25. EXISTING EXC. CULVERT TO REMAIN
26. EXISTING POWER POLE TO REMAIN
27. EXISTING POWER POLE TO BE RELOCATED
28. EXISTING WATER MAIN TO REMAIN
29. EXISTING POWER POLE AND MAST TO BE RELOCATED
30. EXISTING INLET FOR REMOVED AND REPLACED WITH TYPE R INLET
31. EXC. UTILITY BASEMENT
32. EXISTING WATER LINE TO BE REMOVED
33. PROPOSED STORM SEWER INLET (BY OTHERS)
34. PROPOSED STORM SEWER MANHOLE (BY OTHERS)







**ENLARGEMENT-A: BOX CULVERT EXTENSION**  
SCALE: 1"=4'

1. ALL UTILITY LOCATIONS SHOWN ARE BASED ON MAPS PROVIDED BY THE APPROPRIATE UTILITY COMPANY AND FIELD SURFACE EVIDENCE AT THE TIME OF SURVEY AND IS TO BE CONSIDERED AN APPROXIMATE LOCATION ONLY. IT IS THE CONTRACTOR'S RESPONSIBILITY TO FIELD VERIFY THE FIELD LOCATION OF ALL UTILITIES, PUBLIC OR PRIVATE, WHETHER SHOWN ON THE PLANS OR NOT, PRIOR TO CONSTRUCTION. REPORT ANY DISCREPANCIES TO THE ENGINEER PRIOR TO CONSTRUCTION.
2. WHERE A PROPOSED UTILITY CROSSES AN EXISTING UTILITY, IT IS THE CONTRACTOR'S RESPONSIBILITY TO FIELD VERIFY THE HORIZONTAL AND VERTICAL LOCATION OF SUCH EXISTING UTILITY, EITHER THROUGH POT-HOLING OR ALTERNATIVE METHOD. REPORT INFORMATION TO THE ENGINEER PRIOR TO CONSTRUCTION.

BEARINGS ARE BASED ON THE NORTH LINE OF THE NORTHEAST QUARTER OF SECTION 34 AND MONUMENTED ON THE WEST BY A 2.5" ALUMINUM CAP STAMPED "PLS20752" IN A RANGE BOX FOR THE N 1/4 CORNER AND MONUMENTED ON THE EAST BY A 2.5" ALUMINUM CAP STAMPED "PLS12405" FOR THE NE CORNER AND IS ASSUMED TO BEAR S 89°58'01" E.

ELEVATIONS SHOWN ARE NAVD 88 AND ARE DERIVED VIA GPS OBSERVATIONS FROM THE LIECA SMARTNET RTK GPS NETWORK AND GEOID 12B.

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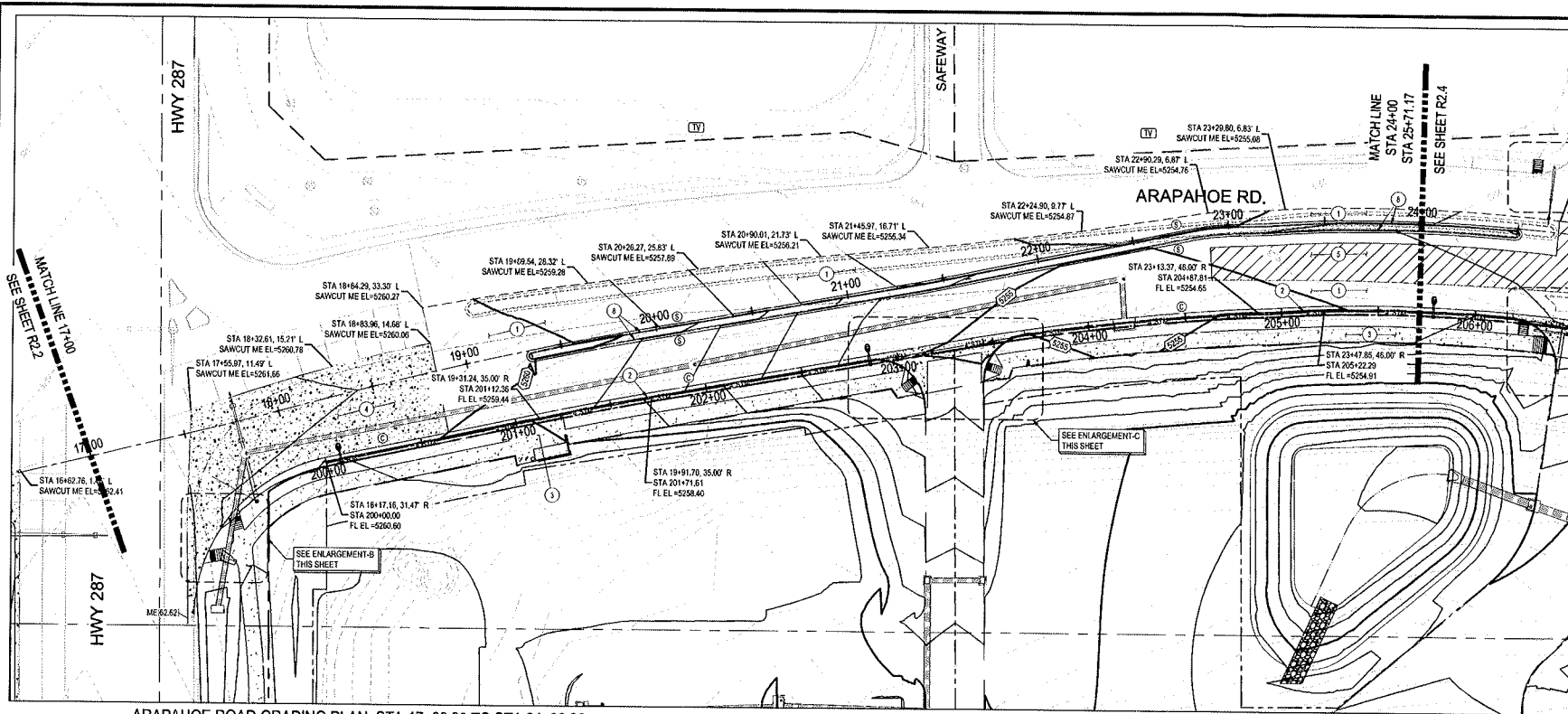
INFRASTRUCTURE CONSTRUCTION  
DOCUMENTS  
NINE MILE CORNER  
EVERGREEN DEVCO INC  
  
HIGHWAY 287 & ARAPAHOE RD.  
ERIE, COLORADO

[illegible]

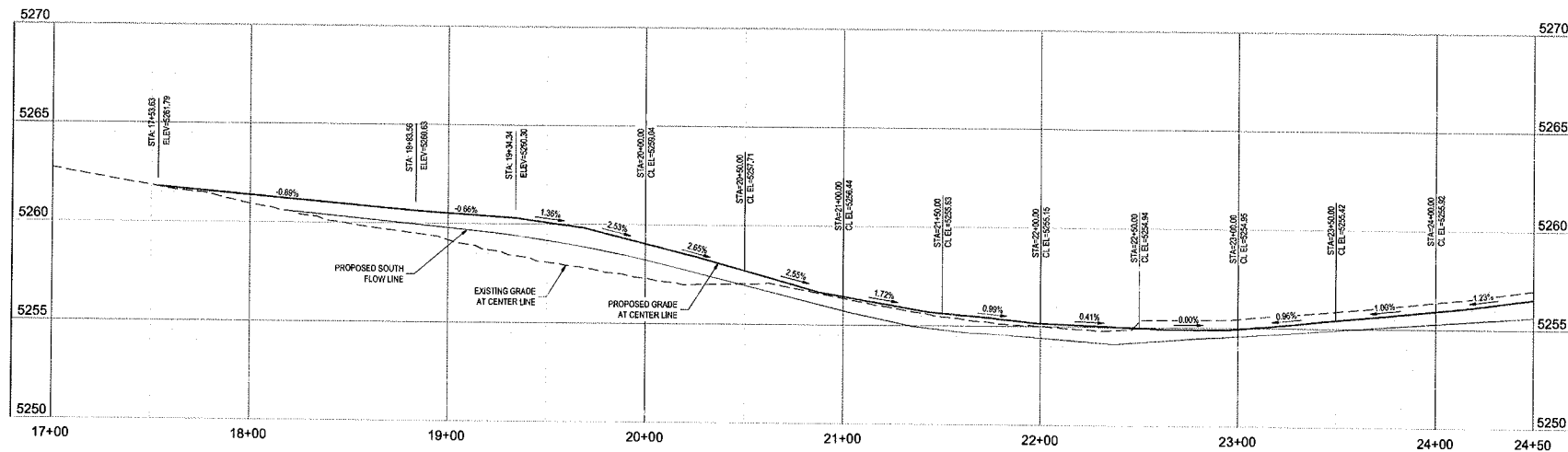
Project No:	ED1000018
Drawn By:	RDG
Checked By:	BSM
Date:	9/3/19

ARAPAHOE ROAD GRADING  
PLAN & PROFILE

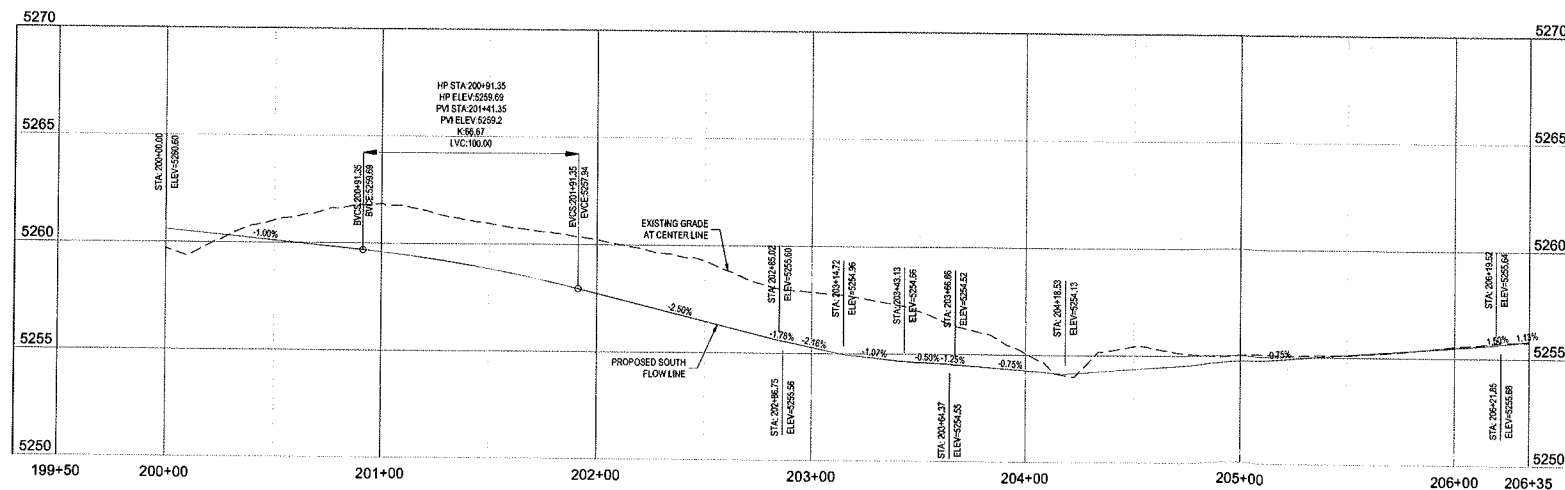
## R2.2



ARAPAHOE ROAD GRADING PLAN: STA 17+00.00 TO STA 24+00.00  
SCALE: 1"=40'



ARAPAHOE ROAD GRADING PROFILE: STA 17+00.00 TO STA 24+00.00  
SCALE: 1"=40' HORIZ, 1"=4' VERT



ARAPAHOE ROAD SOUTH FLOW LINE PROFILE: STA 20+00.00 TO STA 25+71.17  
SCALE: 1"=40' HORIZ, 1"=4' VERT

## GRADING SCHEDULE

- PROPOSED HEAVY DUTY ASPHALT PAVEMENT
- PROPOSED 6' CURB AND 2' GUTTER
- PROPOSED SIDEWALK
- PROPOSED CONCRETE PAVEMENT
- PROPOSED ASPHALT MILL AND OVERLAY
- PROPOSED ADA RAMP
- PROPOSED GUARDRAIL
- PROPOSED 6' CURB AND 2' GUTTER

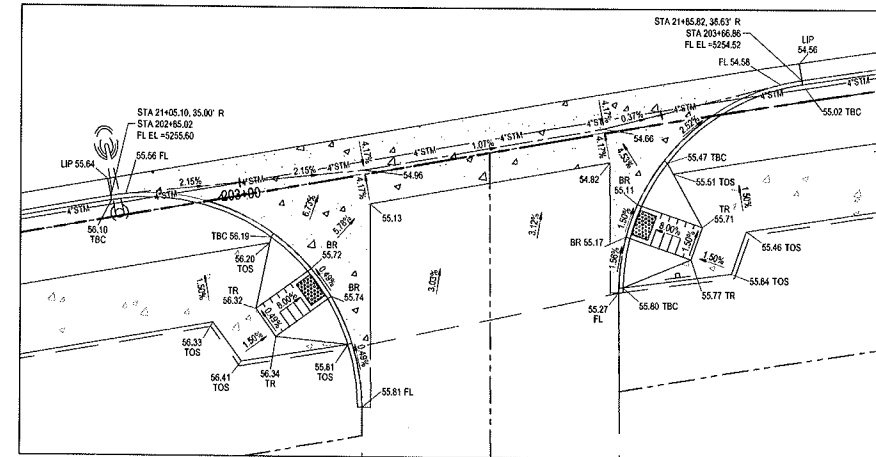


## GRADING LEGEND

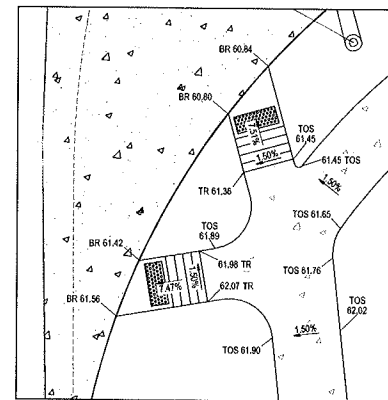
- EXISTING MINOR CONTOUR
- EXISTING MAJOR CONTOUR
- PROPOSED MINOR CONTOUR
- PROPOSED MAJOR CONTOUR
- CATCH CURB AND GUTTER
- SPILL CURB AND GUTTER

## ROADWAY LEGEND

- PROPERTY BOUNDARY LINE
- PROPOSED LOT LINE
- ADJACENT PROPERTY BOUNDARY LINE
- EASEMENT BOUNDARY LINE
- FIRE ACCESS EASEMENT
- SIDEWALK EASEMENT
- LANDSCAPE BUFFER
- EXISTING TO REMAIN
- PROPOSED FUTURE
- PROPOSED NEW
- PROPOSED HEAVY DUTY ASPHALT PAVEMENT
- PROPOSED 2" ASPHALT MILL AND OVERLAY
- PROPOSED CONCRETE PAVEMENT



ENLARGEMENT-C  
SCALE: 1"=10'



ENLARGEMENT-B  
SCALE: 1"=10'

## BASIS OF BEARING

BEARINGS ARE BASED ON THE NORTH LINE OF THE NORTHEAST QUARTER OF SECTION 34 AND MONUMENTED ON THE WEST BY A 2" ALUMINUM CAP STAMPED "PLS2872" IN A RANGE BOX FOR THE 1/4 CORNER AND MONUMENTED ON THE EAST BY A 2" ALUMINUM CAP STAMPED "PLS12405" FOR THE NE CORNER AND IS ASSUMED TO BEAR S 89°50'1" E.

## BENCHMARK

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## CAUTION - NOTICE TO CONTRACTOR

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- WHERE A PROPOSED UTILITY CROSSES AN EXISTING UTILITY, IT IS THE CONTRACTOR'S RESPONSIBILITY TO FIELD VERIFY THE HORIZONTAL AND VERTICAL LOCATION OF SUCH EXISTING UTILITY, EITHER THROUGH POT-HOLING OR ALTERNATIVE METHOD. REPORT INFORMATION TO THE ENGINEER PRIOR TO CONSTRUCTION.

**Galloway**

6162 S. Willow Drive, Suite 320  
Greenwood Village, CO 80111  
303.770.8884  
gallowayus.com

**Evergreen**  
Development | Services | Investments



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INFRASTRUCTURE CONSTRUCTION DOCUMENTS

NINE MILE CORNER  
EVERGREEN DEVCO INC

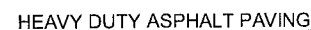
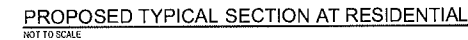
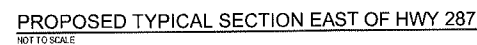
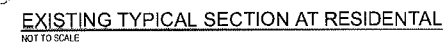
HIGHWAY 287 & ARAPAHOE RD.  
ERIE, COLORADO

#	Date	Issue / Description	Init.
1	11/15/19	4TH INFRASTRUC. SUB.	ACJ
2	01/24/20	5TH INFRASTRUC. SUB.	ACJ
3	02/20/20	6TH INFRASTRUC. SUB.	ACJ
4	03/03/20	7TH INFRASTRUC. SUB.	ACJ
5	03/17/20	FINAL STAMPED INF. SET	BSM

Project No:	ED000018
Drawn By:	RDG
Checked By:	BSM
Date:	9/3/19

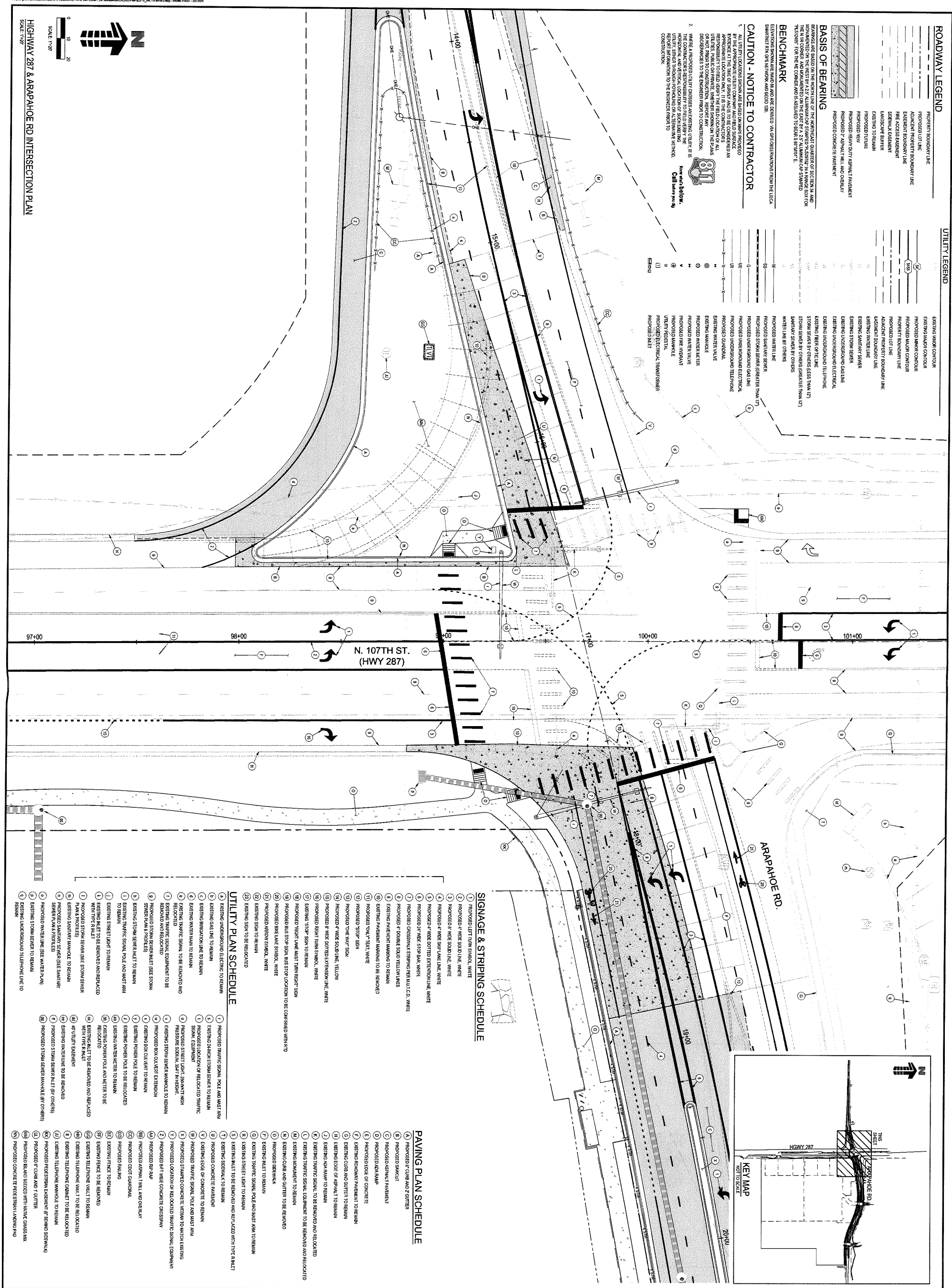
ARAPAHOE ROAD GRADING PLAN & PROFILE

**R2.3**

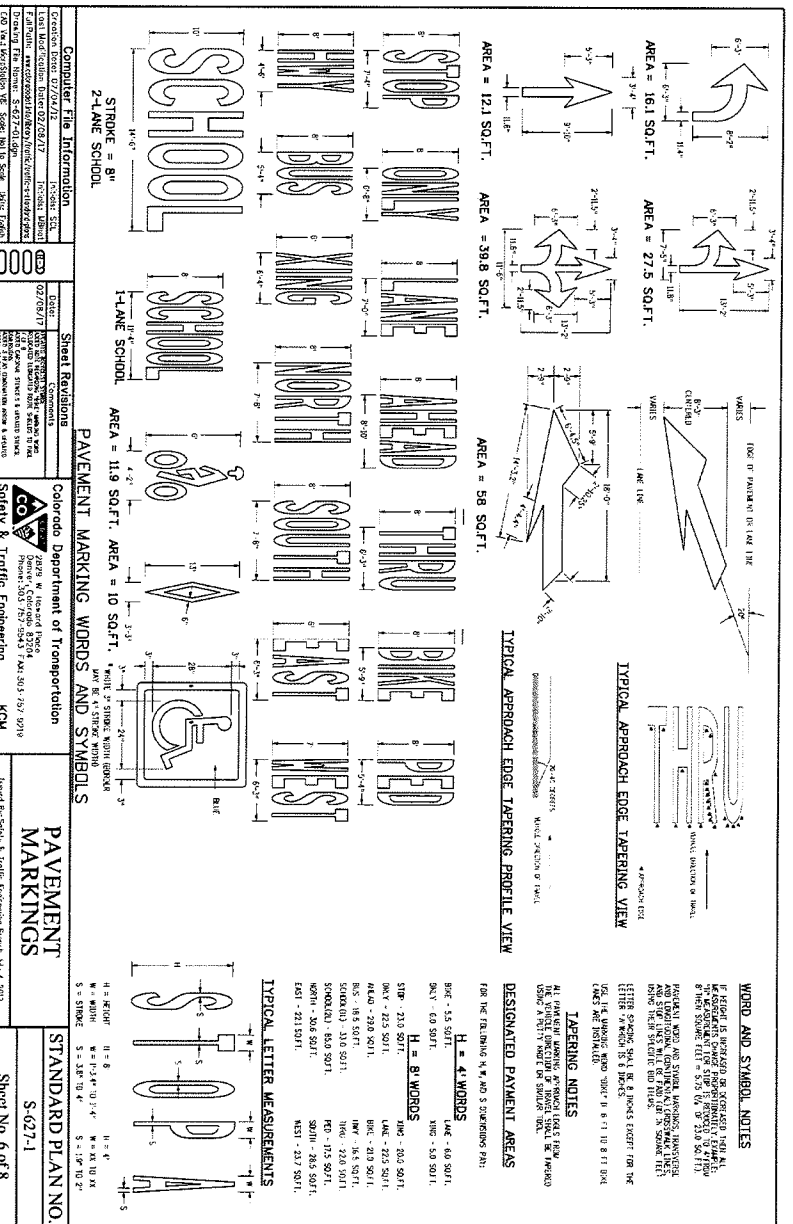


CONTRACTOR SHALL REFERENCE GEOTECHNICAL REPORT BY CIL THOMPSON, INC., FOR PAVEMENT DESIGN AND SOIL PREPARATION REQUIREMENTS.

# R3.1









ARAPAHOE RD.

# R5.5

Journal Pre-proof

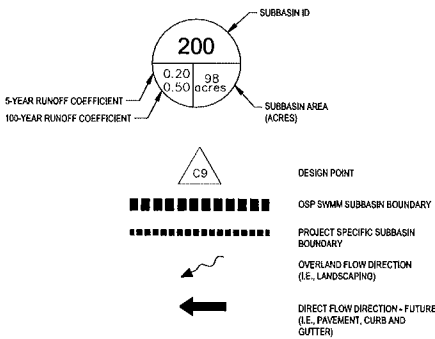
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**LEGEND:**

- PROPOSED MAJOR CONTOUR
- PROPOSED MINOR CONTOUR
- EXISTING MAJOR CONTOUR
- EXISTING MINOR CONTOUR
- EXISTING STORM SEWER
- PROPOSED STORM SEWER
- PROPOSED STORM INLET
- PROPOSED RIGHT-OF-WAY
- FUTURE RIGHT-OF-WAY
- PROPOSED LOTLINE
- FUTURE LOTLINE
- EASEMENT LINE



**DRAINAGE SYMBOLS:**



**NOTES:**

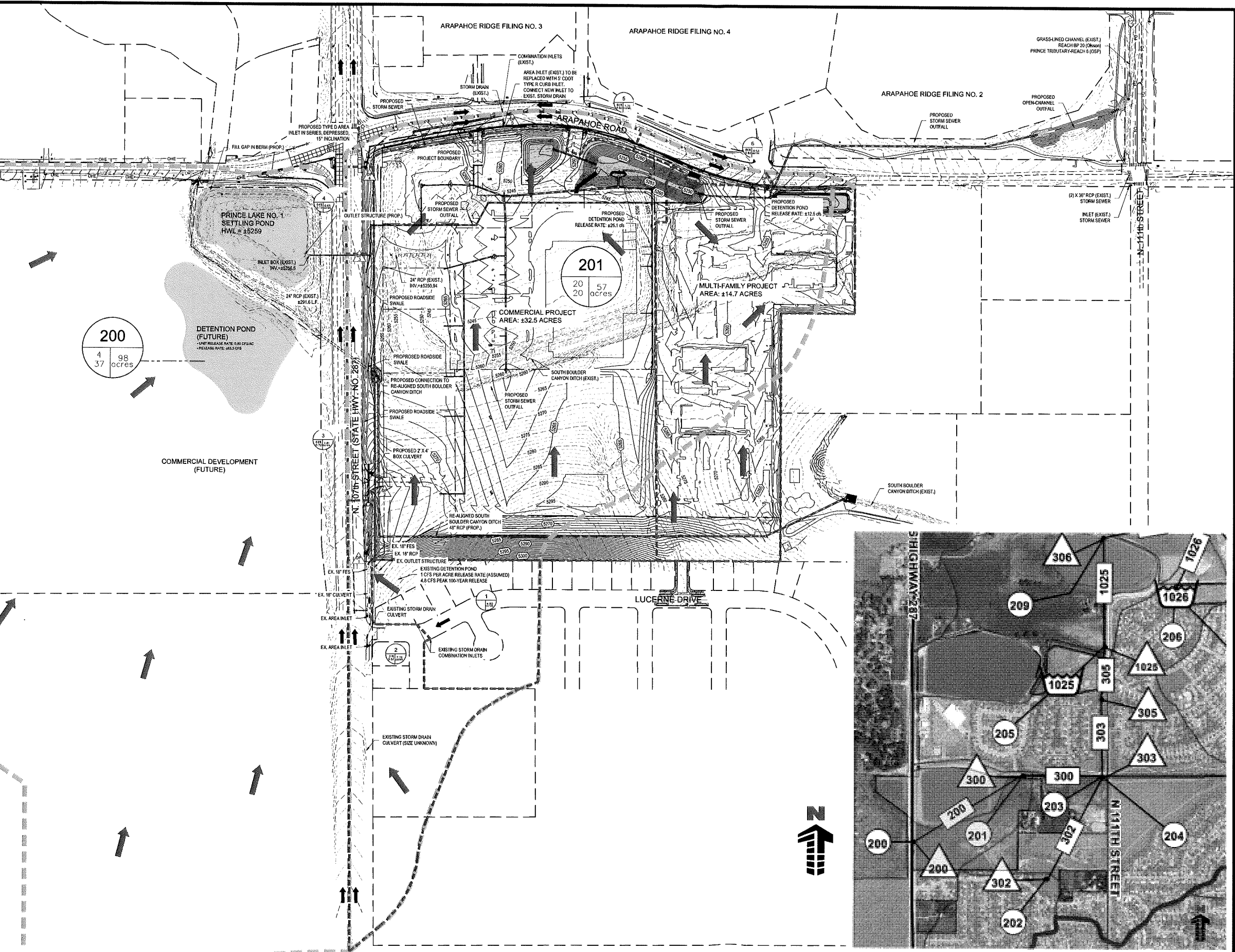
- 1 DETENTION POND FOOTPRINT (PROPOSED CONDITION)
- 100-YEAR

Project No: 05/17/2017  
Drawn By: HHF  
Checked By: HHF

**NINE MILE CORNER**  
ERIE, COLORADO  
N. 107th Street and Arapahoe Road

DR01

1 of 1



**FINAL DRAINAGE PLAN**  
**NINE MILE CORNER | ERIE, CO**  
SCALE - 1" = 150'

Inset from Figure B-6  
Town of Erie Outfall Systems Plan (West of Coal Creek)  
RESPEC Consulting & Services, January 2014

SCALE - NTS

# STATE DEPARTMENT OF HIGHWAYS

## DIVISION OF HIGHWAYS—STATE OF COLORADO

## CONVENTIONAL SIGNS

County Line  
Township or Range Line  
Land Lines  
Property or Tract Line  
City Limits  
Railroad  
Existing Road  
New Road

Control of Access  
Access denied by Dead End  
Right of Way Line  
Protected by Freeway (Virgin Location)  
Top of Cuts  
Toe of Fills  
Barbed Wire Fence  
Chain Link Fence  
Woven Wire—Combination Fence  
Snow Fence  
Wood Fence  
Deer Fence

Trees  
Deciduous  
Coniferous

Te. & Tel. Lines  
Electric Lines  
Elec. Trans. Tower  
Buried Tel. Cable  
Buried Electric Cable  
Gas Main  
Oil Main  
Water Main  
Sanitary Sewers  
Storm Sewers

Road Approaches  
Section Corner  
Center of Section

Culverts & Drains  
Proposed  
Existing  
Proposed  
Existing (To be Dashed)

Bridges  
Existing (To be Dashed)

### PLAN AND PROFILE OF PROPOSED FEDERAL AID PROJECT NO. FC-HES 007-2(5) STATE HIGHWAY NO 7 & 287 BOULDER COUNTY

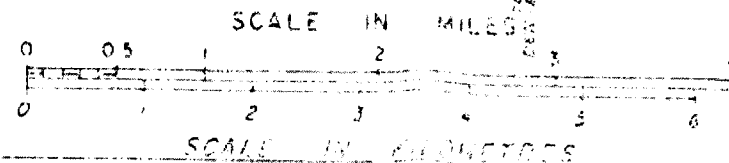
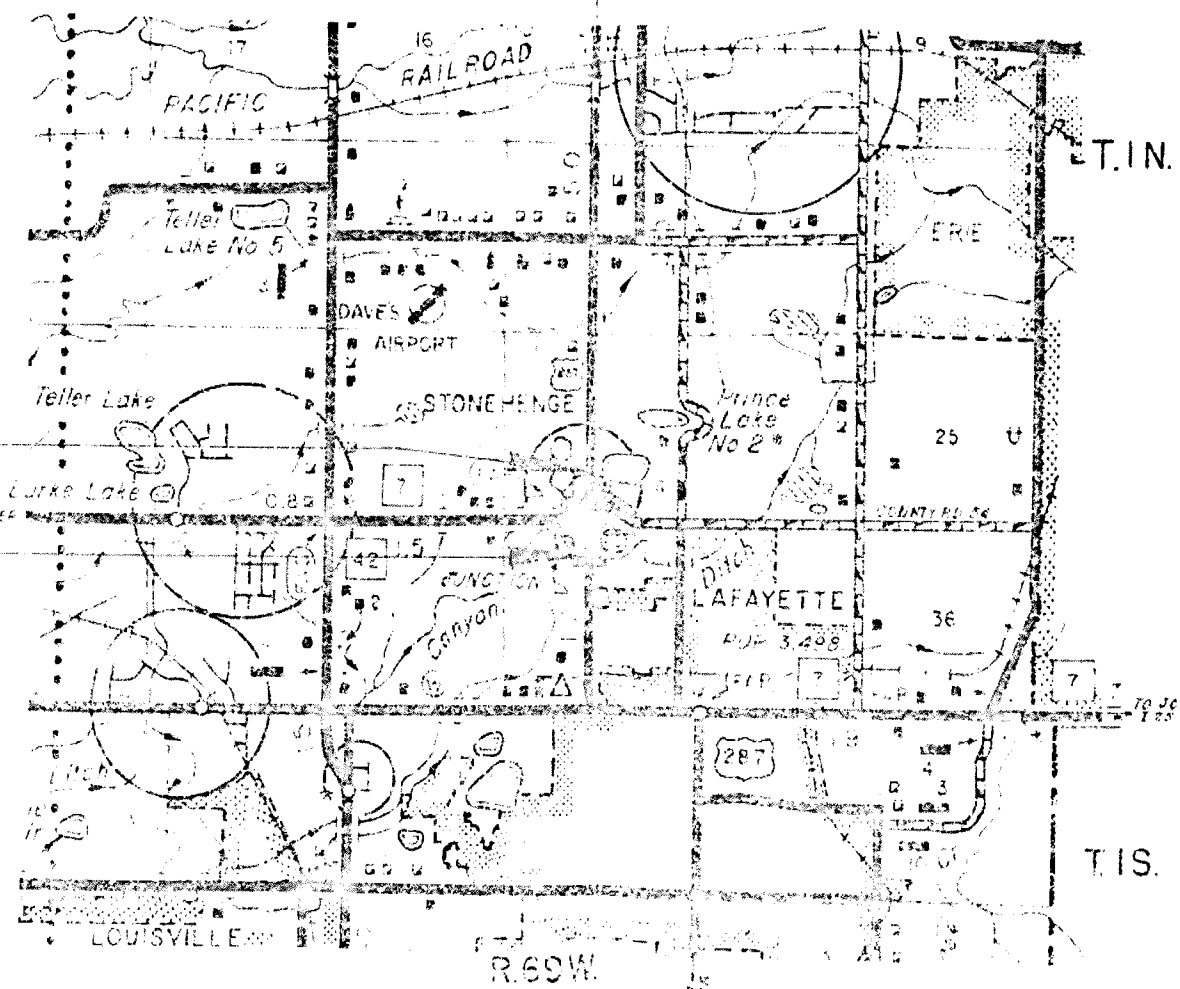
## RIGHT OF WAY

SCALE OF ORIGINAL DRAWINGS  
PLAN SHEETS 1" = 50'  
DETAIL SHEETS 1" = 10'  
OWNERSHIP MAPS 1" = 200'

ROW LENGTH OF PROJECT = 0.363 MILES

BEGIN ROW PROJECT FC-HES 007-2(5)  
STATION 516+46.2 (Proj. S.H. 287)

END ROW PROJECT FC-HES 007-2(5)  
STATION 535+44.2 (Proj. S.H. 287)



PROJECT NO.	SECTION	SHEET NO.	TOTAL SHEETS
FC-HES 007-2(5)			
RIGHT OF WAY			
SHEET NO. 7 & S.H. 287			

## INDEX OF SHEETS

- 1 TITLE SHEET
- 2-2A TABULATION OF PROPERTIES
- 3-7 PLAN SHEETS
- 8 OWNERSHIP MAP

DIVISION OF HIGHWAYS	
APPROVED	
SUPV. ENGINEER	DATE

DEPARTMENT OF TRANSPORTATION FEDERAL HIGHWAY ADMINISTRATION	
APPROVED	
DIVISION ADMINISTRATOR	DATE





TIN, R 69W, 6TH PIA

REVISIONS	DATE	BY	REASON
1	11-1-78	J. H. HARRIS	INITIAL
2	11-1-78	J. H. HARRIS	INITIAL
3	11-1-78	J. H. HARRIS	INITIAL
4	11-1-78	J. H. HARRIS	INITIAL
5	11-1-78	J. H. HARRIS	INITIAL

RIGHT OF WAY  
Sec 27 & 28 SH 287

① EAP. J. H. HARRIS  
EL PASO TOWN OF ERIE

⑦ TOWN OF ERIE  
EL PASO

UTILITY OWNERS  
Mountain Bell  
Public Service  
Left Hand Water District

SE 1/4 SEC 27  
TIN R 69W

ERIE  
LAKE

STORAGE POND  
DOMESTIC WATER SUPPLY

Projected BL  
Δ = 15° 30' LI  
D = 6° 00'  
T = 130.0  
L = 238.3  
R = 954.9

Sta 516+31.3 (Proj. SH 287)  
Sta 110+00.4 (Proj. BL)

⑦ TOWN OF ERIE  
EL PASO



SW 1/4 Sec 27  
TIN R 69W

STONEHENGE

SUBDIVISION

STONEHENGE CIRCLE

PLACE

BEGIN ROW PROJECT TC-HES 007-2151  
STATION 516+46.2 (Proj. SH 287)  
Sta 516+46.2 (Proj. SH 287)  
Sta 110+00.0 (Proj. BL)

STILLWATER

505

510

515

520

10 LAMMONT US 287

2 GAS LINE PUBLIC SERVICE

6" WATER LINE LEFT HAND WATER DIST.  
MOUNTAIN BELL TELEPHONE

18" x 17" STEEL PIPE  
IN DITCH

TEL. P.D.  
STAND P.D.

18" x 50" C&P  
w/ 10' B&B  
15" x 20" C&P S&P DRAIN  
w/ 10' B&B

15" x 16" BOLLER TUBE  
S&P DRAIN  
15" x 6" C&P  
S&P DRAIN

18" x 5" C&P  
w/ 10' B&B (H&W-AP)

FEDERAL ROAD DISTRICT	BL. SIGN	PROJ. NO.	SHEET NO.	TOTAL SHEETS
COLO. 10	COLO. 10	FC-HES 007-2(5)	4	8

REVISIONS				
NO.	BY	DATE	DESCRIPTION	APPROVED
1	FC	10/1/77	REVISED	FC
2	FC	10/1/77	REVISED	FC
3	FC	10/1/77	REVISED	FC
4	FC	10/1/77	REVISED	FC
5	FC	10/1/77	REVISED	FC
6	FC	10/1/77	REVISED	FC
7	FC	10/1/77	REVISED	FC
8	FC	10/1/77	REVISED	FC
9	FC	10/1/77	REVISED	FC
10	FC	10/1/77	REVISED	FC

RIGHT OF WAY  
Jct. S.H. 7 & S.H. 287

SE<sup>4</sup> SEC 27  
T 1 N, R 69 W

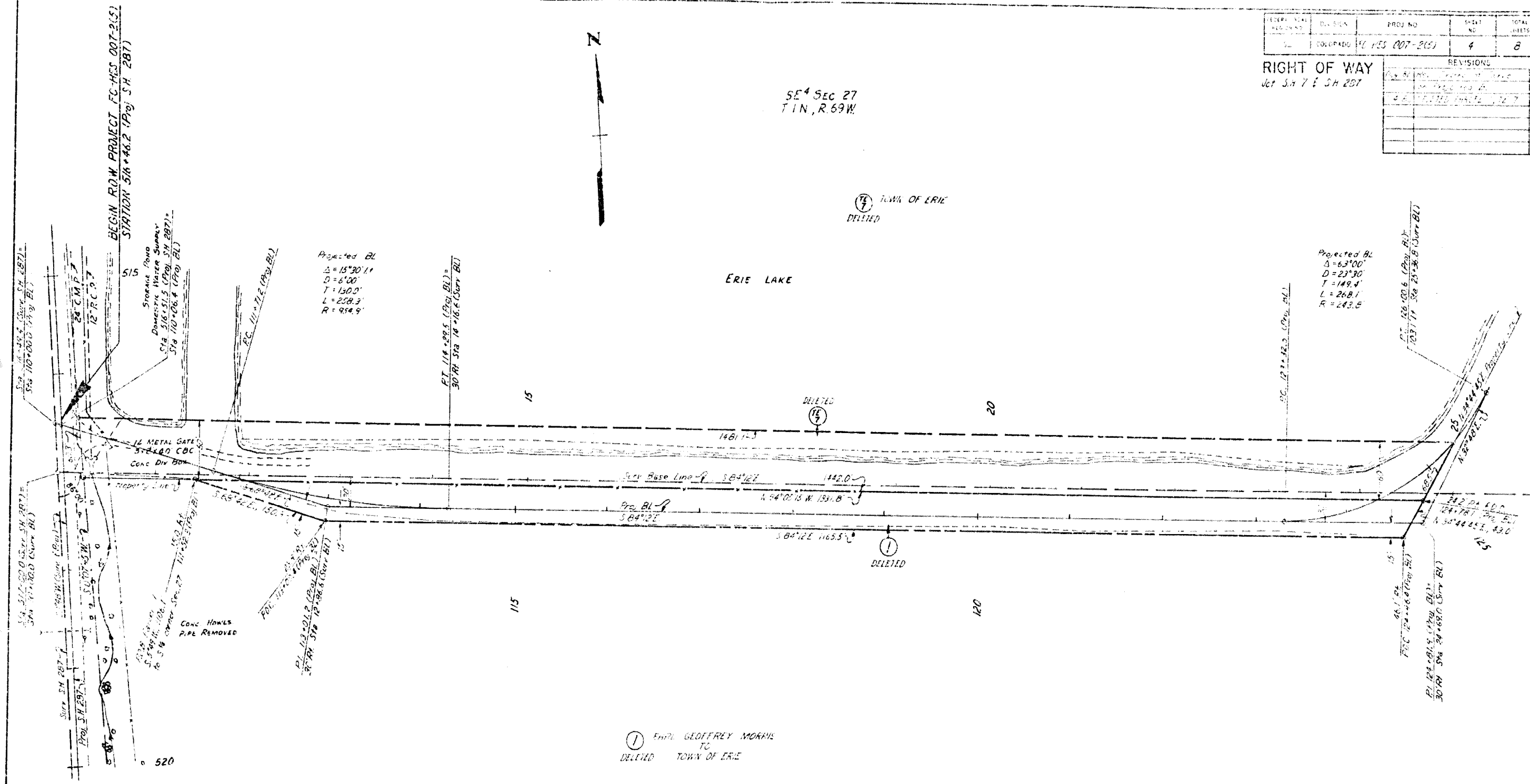
① TOWN OF ERIE  
DELETED

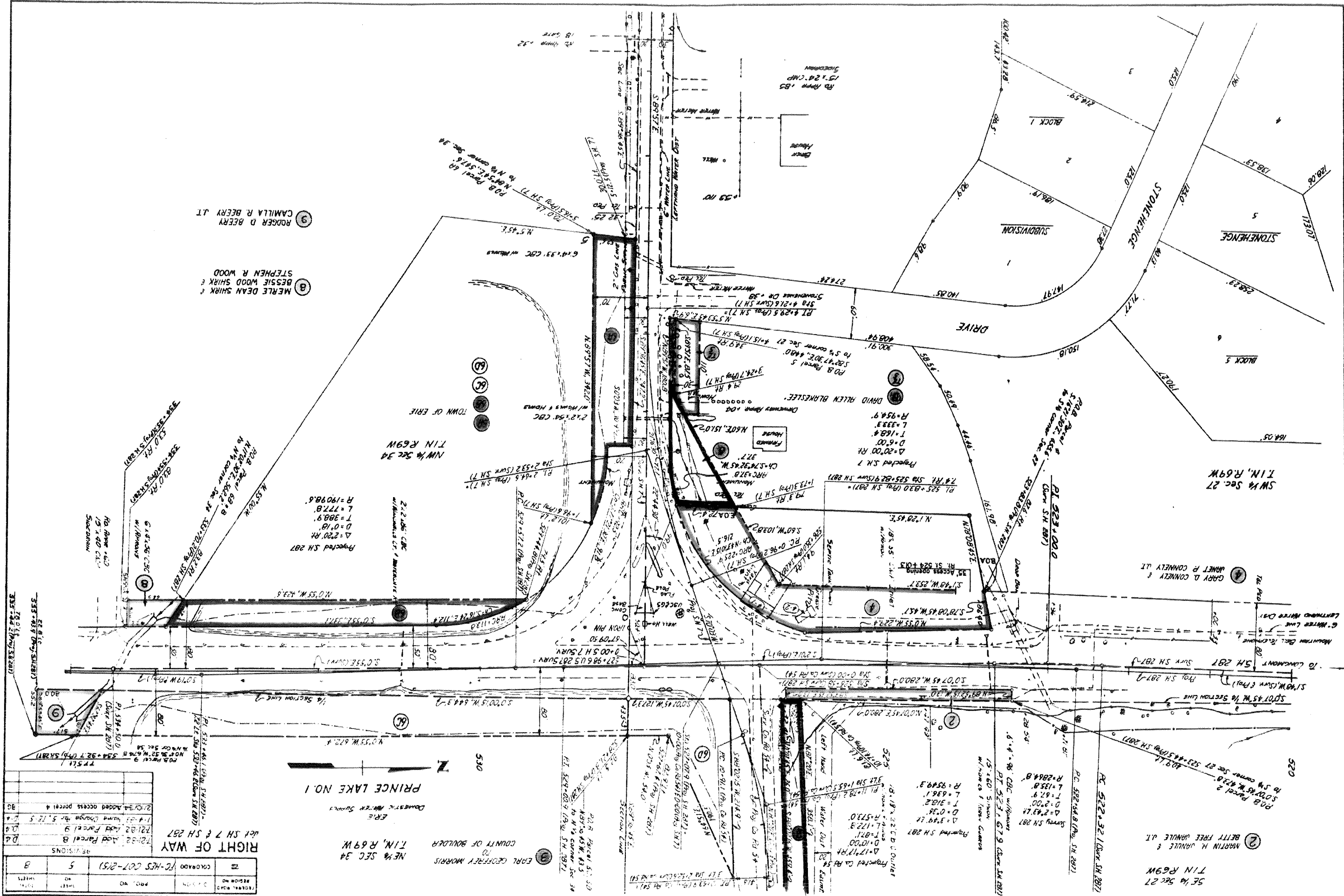
ERIE LAKE

Projected BL  
Δ = 15°30'11"  
D = 6'00"  
T = 150.0'  
L = 258.3'  
R = 958.9'

Projected BL  
Δ = 63°00'  
D = 23'30"  
T = 149.4'  
L = 268.1'  
R = 243.5'

① ENRL GLOFFREY MORRIS  
FC  
DELETED TOWN OF ERIE





REVISIONS	NO.	DATE	DESCRIPTION
1	5	12-15-21	Added Parcel 9
2	5	12-15-21	Added Parcel 10
3	5	12-15-21	Added Parcel 11
4	5	12-15-21	Added Parcel 12
5	5	12-15-21	Added Parcel 13
6	5	12-15-21	Added Parcel 14
7	5	12-15-21	Added Parcel 15
8	5	12-15-21	Added Parcel 16
9	5	12-15-21	Added Parcel 17
10	5	12-15-21	Added Parcel 18

T.M. R. 54 W. 5TH PM.

REGIONAL NO.	PROJECT NO.	SHEET NO.	TOTAL SHEETS
1	COLORED "C-1115" (27-21-51)	5	8

REVISIONS	

RIGHT OF WAY  
Vet. SH. 7 & SH. 287

SE 1/4 Sec. 27

LAND GEORGE MORRIS  
TO  
COUNTY OF BOULDER

Projected Co. Rd. 54  
 $\Delta = 17^{\circ}17'17''$   
 $D = 10^{\circ}00'$   
 $T = 87.1'$   
 $L = 172.6'$   
 $R = 573.0'$

Let. Hand Water Dist. Easmt.

Projected Co. Rd. 54  
Survey Co. Rd. 54  
PT. 12+53.7 (Proj. Co. Rd. 54)  
S. 1/4 Sec. 27  
P.C. 10+91.1 (Proj. Co. Rd. 54)  
N. 63°44'30" E

N. 63°44'30" E  
Section Line

PRINCE LAKE NO. 1

NE 1/4 Sec. 24

Survey Co. Rd. 54  
 $\Delta = 46^{\circ}59'30''$   
 $D = 22^{\circ}00'$   
 $T = 113.2'$   
 $L = 213.6'$   
 $R = 260.4'$

Survey Co. Rd. 54  
 $\Delta = 46^{\circ}59'30''$   
 $D = 22^{\circ}00'$   
 $T = 113.2'$   
 $L = 213.6'$   
 $R = 260.4'$

P. 12+23.6  
Ch. 15 No. 38

Undersized 18" Pipe  
16" Field Pipe  
Concrete Steel at 16" B.W. Gate

15'-55" CSD  
D. 60"



(60) TOWN OF ERIC  
(60)

2100' Co. Rd. 54 Survey  
326+58 Survey US 287  
P. 1+22.2 (Proj. Co. Rd. 54)  
S. 1/4 Sec. 27  
P. 1+22.2 (Proj. Co. Rd. 54)  
S. 1/4 Sec. 27

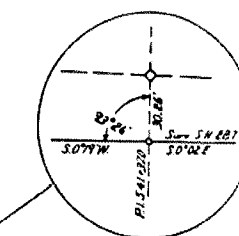
SH. 287  
S. 1/4 Sec. 27  
S. 1/4 Sec. 27  
S. 1/4 Sec. 27

530



RIGHT OF WAY  
Jct. S.H. 7 & S.H. 26  
OWNERSHIP MAP

LEONARD L. LANHAM &  
NINA E. LANHAM J.T.  
10960 ARAPAHOE AVE  
LAFAYETTE, COLO. 80026  
Pt. N<sup>2</sup> NE<sup>4</sup> Sec. 34



ORIGINAL SCALE: 1"=200'